



Glen Eira City Council

Planning Enquiries

Phone: 03 9524 3333

Web: <http://www.gleneira.vic.gov.au>

Office Use Only

Application No.:

ADVERTISED PLAN

Date Lodged: 29 / 01 / 2015

# Application for Planning Permit

This copied document is made available for the sole purpose of providing information and advice as part of the planning process under the Planning and Environment Act 1987. The document is not to be used for any other purpose.

If you need help to complete this form, read [How to complete the Application for Planning Permit form](#).

**A** Any material submitted with this application, including plans and personal information, will be made available for public viewing, including electronically, and copies may be made for interested parties for the purpose of enabling consideration and review as part of a planning process under the Planning and Environment Act 1987. If you have any concerns, please contact Council's planning department.

**A** Questions marked with an asterisk (\*) are mandatory and must be completed.

**A** If the space provided on the form is insufficient, attach a separate sheet.

Clear Form

## The Land **1** **1** Address of the land. Complete the Street Address and one of the Formal Land Descriptions.

Street Address \*

Unit No.:	St. No.: 134, 136 & 138	St. Name: McKinnon Rd
Suburb/Locality: McKinnon		Postcode: 3204

Formal Land Description \*  
Complete either A or B.

**A** This information can be found on the certificate of title.

A	Lot No.: 1	<input type="radio"/> Lodged Plan	<input checked="" type="radio"/> Title Plan	<input type="radio"/> Plan of Subdivision	No.: 571838N
OR					
B	Crown Allotment No.:	Section No.:			
Parish/Township Name:					

Remove Address

Formal Land Description \*  
Complete either A or B.

**A** This information can be found on the certificate of title.

A	Lot No.: 1 & 2	<input type="radio"/> Lodged Plan	<input checked="" type="radio"/> Title Plan	<input type="radio"/> Plan of Subdivision	No.: 851207R
OR					
B	Crown Allotment No.:	Section No.:			
Parish/Township Name:					

Remove Address

Formal Land Description \*  
Complete either A or B.

**A** This information can be found on the certificate of title.

A	Lot No.: 29	<input type="radio"/> Lodged Plan	<input type="radio"/> Title Plan	<input checked="" type="radio"/> Plan of Subdivision	No.: 003079
OR					
B	Crown Allotment No.:	Section No.:			
Parish/Township Name: (Block A)					

Remove Address

If this application relates to more than one address, please click this button and enter relevant details.

Add Address

## The Proposal **A** You must give full details of your proposal and attach the information required to assess the application. Insufficient or unclear information will delay your application.

2 For what use, development or other matter do you require a permit? \*


If you need help about the proposal, read:  
[How to Complete the Application for Planning Permit Form](#)

Buildings and works being for dwellings and a reduction of car parking under Clause 52.06 of the Glen Eira Planning Scheme

☒ Provide additional information on the proposal, including: plans and elevations; any information required by the planning scheme, requested by Council or outlined in a Council planning permit checklist; and if required, a description of the likely effect of the proposal.

3 Estimated cost of development for which the permit is required \*

Cost \$3,000,000

 You may be required to verify this estimate. Insert '0' if no development is proposed.

If the application is for land within metropolitan Melbourne (as defined in section 3 of the Planning and Environment Act 1987) and the estimated cost of the development exceeds \$1 million (adjusted annually by CPI) the Metropolitan Planning Levy must be paid to the State Revenue Office and a current levy certificate must be submitted with the application. Visit [www.sro.vic.gov.au](http://www.sro.vic.gov.au) for information.

## Existing Conditions

4 Describe how the land is used and developed now \*

eg. vacant, three dwellings, medical centre with two practitioners, licensed restaurant with 80 seats, grazing.

single dwellings

☒ Provide a plan of the existing conditions. Photos are also helpful.

## Title Information

5 Encumbrances on title \*

If you need help about the title, read:  
[How to complete the Application for Planning Permit form](#)

Does the proposal breach, in any way, an encumbrance or title such as a restrictive covenant, section 173 agreement or other obligation such as an easement or building envelope?

- ☐ Yes. (If 'yes' contact Council for advice on how to proceed before continuing with this application.)  
☒ No  
☐ No: applicable (no such encumbrance applies).

☒ Provide a full, current copy of the title for each individual parcel of land forming the subject site. (The title includes: the covering 'register search statement', the title diagram and the associated title documents, known as 'instruments', eg. restrictive covenants.)

## Applicant and Owner Details

6 Provide details of the applicant and the owner of the land.

**Applicant \***

The person who wants the permit.

Where the preferred contact person for the application is different from the applicant, provide the details of that person.

Name:

Title:

First Name:

Surname:

Organisation (if applicable): AC Welland McKinnon Pty Ltd - C/- G2 Urban Planning

Postal Address:

If it is a P.O. Box, enter the details here:

Unit No.:

St. No.: 670

St. Name: Mt Alexander Road

Suburb/Locality: Moonee Ponds

State: VIC

Postcode: 3039

Contact person's details \*

Same as applicant (if so, go to 'contact information') ☐

Name:

Title:

First Name: Damian

Surname: Loughnan

Organisation (if applicable): G2 Urban Planning

Postal Address:

If it is a P.O. Box, enter the details here:

Unit No.:

St. No.: 670

St. Name: Mt Alexander Road

Suburb/Locality: Moonee Ponds

State: VIC

Postcode: 3039



Please provide at least one contact phone number \*

#### Contact information

Business Phone: 9372 0401

Email: gap@gattini.com.au

Mobile Phone: 0400 539 762

Fax: 9375 2862

#### Owner \*

The person or organisation who owns the land

Where the owner is different from the applicant, provide the details of that person or organisation.

Name:

Same as applicant ☐

Title:

First Name:

Surname:

Organisation (if applicable): AC Welland MBM Pty Ltd

Postal Address:

If it is a P.O. Box, enter the details here:

Unit No.

St. No.: Level 9 550

St. Name: Bourke St

Suburb/Locality: Melbourne

State: VIC

Postcode: 3000

Owner's Signature (Optional):

Date:

day / month / year

## Declaration **1**

⑦ This form must be signed by the applicant \*

⚠ Remember it is against the law to provide false or misleading information, which could result in a heavy fine and cancellation of the permit.

I declare that I am the applicant, and that all the information in this application is true and correct, and the owner (if not myself) has been notified of the permit application.

Signature:

Date: 15 Dec 2016

day / month / year

## Need help with the Application?

If you need help to complete this form, read [How to complete the Application for Planning Permit form](#).  
General information about the planning process is available at [www.delwp.vic.gov.au/planning](http://www.delwp.vic.gov.au/planning)

Contact Council's planning department to discuss the specific requirements for this application and obtain a planning permit checklist. Insufficient or unclear information may delay your application.

8 Has there been a pre-application meeting with a Council planning officer?

☐ No ☒ Yes

If 'yes', with whom? Adam Stark & Michael Henderson

Date: 25 Oct 2016


day / month / year

## Checklist

9 Have you:

☐ Filled in the form completely?

☐ Paid or included the application fee?

 Most applications require a fee to be paid. Contact Council to determine the appropriate fee.

☒ Provided all necessary supporting information and documents?

☐ A full, current copy of site information for each individual parcel of land forming the subject site

☐ A plan of existing conditions

☐ Plans showing the layout and details of the proposal

☐ Any information required by the planning scheme, requested by council or outlined in a council planning permit checklist

☐ If required, a description of the likely effect of the proposal (eg traffic, noise, environmental impacts).

☐ If applicable, a current Metropolitan Planning Levy certificate (a levy certificate expires 90 days after the day on which it is issued by the State Revenue Office and then cannot be used). Failure to comply means the application is void.

☐ Completed the relevant Council planning permit checklist?

☐ Signed the declaration (section 7)?

## Lodgement

Lodge the completed and signed form, the fee payment and all documents with:

Glen Eira City Council  
PO Box 42 Caulfield South VIC 3162  
Cnr Hawthorn & Glen Eira Roads Caulfield VIC 3162

Contact information:

Telephone: 03 9524 3333

Email: [mail@gleneira.vic.gov.au](mailto:mail@gleneira.vic.gov.au)

DX: 37089

Deliver application in person, by fax, or by post:

Print Form

Make sure you deliver any required supporting information and necessary payment when you deliver this form to the above mentioned address. This is usually your local council but can sometimes be the Minister for Planning or another body.

Save Form:

Save Form To  
Your Computer

You can save this application form to your computer to complete or review later or email it to others to complete relevant sections.

# Metropolitan Planning Levy (MPL)

**Certificate**

**ADVERTISED PLAN**

AC Welland McKinnon Pty Ltd

Suite 1212, 1 Queens Rd

Melbourne VIC 3004

Certificate Number: MPLCERT4364

Issue Date: 7 December 2016

Expiry Date: 7 March 2017

## PART 1 - APPLICANT DETAILS

Details of person who applied for this Certificate:

Name: AC Welland McKinnon Pty Ltd

Address: Suite 1212, 1 Queens Rd  
Melbourne VIC 3004

## PART 2 - LEVIABLE LAND DETAILS

Address of land to which the Metropolitan Planning Levy applies:

Street Address: 134, 136&138 McKinnon Road  
McKinnon VIC 3204

Formal Land Description:

Vol/Folio: 07839 / 039

Lot/Plan: 1 / TP571838N

Block/Subdivision:

Crown Reference:

Other:

Municipality: Glen Eira City Council

Estimated Cost of Development: \$6,000,000

## PART 3 - MPL PAYMENT DETAILS

MPL Application ID: MPL4364

MPL Paid: \$7,800.00

MPL Payment Date: 5 December 2016

## PART 4 - CERTIFICATION

The Commissioner of State Revenue confirms that the whole of the amount of the MPL has been paid in respect of the estimated cost of development.

Paul Broderick  
Commissioner of State Revenue



## PART 5 – EXPLANATORY NOTES

### General

- The Metropolitan Planning Levy (MPL) is imposed for the privilege of making a leviable planning permit application.
- A leviable planning permit application is an application made to a responsible authority or planning authority under sections 47 and 96A of the *Planning and Environment Act 1987* (PEA) for a permit required for the development of land in metropolitan Melbourne, where the estimated cost of the development for which the permit is required exceeds the threshold amount (see MPL threshold amount).
- As a statutory requirement of making a leviable planning permit application, the applicant must give the responsible authority or planning authority a current MPL Certificate. The estimated cost of development stated in the MPL Certificate must be equal to or greater than the estimated cost of the development stated in the leviable planning permit application. If an applicant fails to comply with this requirement, the application for the leviable planning permit is void.
- The applicant for the leviable planning permit application is liable for the MPL.
- The Commissioner of State Revenue (Commissioner) has the general administration of the MPL.

### MPL threshold amount

- The threshold amount is \$1 million for the 2015-2016 financial year.
- For the financial year beginning on 1 July 2016 and each subsequent financial year, the Consumer Price Indexed (CPI) adjusted threshold amount will be calculated in accordance with section 96R of the PEA.
- On or before 31 May each year, the Commissioner must publish the CPI adjusted threshold amount for the following financial year on the SRO website.

### How MPL is calculated

- The amount of MPL is \$1.30 for every \$1000 of the estimated cost of the development for which the leviable planning permit is required.
- If the estimated cost of the development for which the leviable planning permit is required is not a multiple of \$1000, the estimated cost is to be rounded up or down to the nearest \$1000 (and, if the amount by which it is to be rounded is \$500, it is to be rounded up).

### Notification and Payment of MPL to the Commissioner

- Before making a leviable planning permit application, the applicant must submit a completed Application for Metropolitan Planning Levy (MPL) Certificate and pay the whole MPL amount to the Commissioner. This Application must state the estimated cost of the development and any other information required by the Commissioner.
- If, after the Commissioner has issued a MPL Certificate which has not expired (see MPL Certificate), and the estimated cost of the development increases before the leviable planning permit application is made, the applicant must submit an Application for Metropolitan Planning Levy (MPL) Certificate (Revised) and pay the whole additional MPL amount to the Commissioner. This revised Application must state the increased estimated cost of the development and any other information required by the Commissioner.

### MPL Certificate

- The Commissioner must issue a MPL Certificate if he is satisfied that the whole amount of the MPL has been paid in respect of the estimated cost of the development.
- Subject to section 96U(3) of the PEA, a MPL Certificate expires 90 days after the day on which it is issued.

### Revised MPL Certificate

- The Commissioner must issue a revised MPL Certificate if:
  - the Commissioner has issued a MPL Certificate, which has not expired;
  - the estimated cost of the development increases before the application for a leviable planning permit is made; and
  - he is satisfied that the whole amount of the MPL has been paid in respect of the increased estimated cost of the development.
- The Commissioner may also issue a revised MPL Certificate to:
  - Correct any error in the information listed in the MPL Certificate (except the estimated cost of development as explained below), or
  - the estimated cost of the development stated in the MPL Certificate is different from the estimated cost of the development stated in the Application for Metropolitan Planning Levy (MPL) Certificate lodged by the applicant.
- A revised MPL Certificate expires 90 days after the day on which it is issued.

### Refund of MPL

- The only circumstance under which a person who has paid a MPL is entitled to a refund is where there has been a mathematical error in calculating the amount of the MPL by reference to the estimated cost of the development stated in the original or revised Application for Metropolitan Planning Levy (MPL) Certificate. Other than that, a person who has paid a MPL is not entitled to a refund of the whole or any part of the MPL.

### Certificate number

- The Certificate number is on the top right corner on the front of this Certificate.
- Quoting this Certificate number will give you access to information about this Certificate and enable you to enquire about your application by phone.
- You should quote this number in any correspondence.

For more Metropolitan Planning Levy information please contact the State Revenue Office:

<b>Mail</b> State Revenue Office, GPO Box 4376, MELBOURNE VIC 3001 or DX260050 Melbourne	<b>Internet</b> <a href="http://www.sro.vic.gov.au">www.sro.vic.gov.au</a> <b>Email</b> <a href="mailto:mpl@sro.vic.gov.au">mpl@sro.vic.gov.au</a> <b>Phone</b> 13 21 61 (local call cost) <b>Fax</b> 03 9628 6856
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**REGISTER SEARCH STATEMENT (Title Search) Transfer of  
Land Act 1958**

Page 1 of 1

VOLUME 07839 FOLIO 039

Security no : 124063596357D

Produced 02/12/2016 03:48 pm

**ADVERTISED PLAN**

**3 OF 29**

**LAND DESCRIPTION**

Lot 1 on Title Plan 571838N.  
PARENT TITLE Volume 02323 Folio 512  
Created by instrument 2538165 28/01/1953

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**REGISTERED PROPRIETOR**

Estate Fee Simple

Sole Proprietor

AC WELLAND MBM PTY LTD of LEVEL 9 550 BOURKE STREET MELBOURNE VIC 3000  
AM360180J 26/11/2015

**ENCUMBRANCES, CAVEATS AND NOTICES**

MORTGAGE AM360181G 26/11/2015  
NATIONAL AUSTRALIA BANK LTD

Any encumbrances created by Section 98 Transfer of Land Act 1958 or Section 24 Subdivision Act 1988 and any other encumbrances shown or entered on the plan set out under DIAGRAM LOCATION below.

**DIAGRAM LOCATION**

SEE TP571838N FOR FURTHER DETAILS AND BOUNDARIES

**ACTIVITY IN THE LAST 125 DAYS**

NIL

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: 134 MCKINNON ROAD MCKINNON VIC 3204

**ADMINISTRATIVE NOTICES**

NIL

eCT Control 160889P NATIONAL AUSTRALIA BANK LIMITED  
Effective from 23/10/2015

DOCUMENT END



[illegible]





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**REGISTER SEARCH STATEMENT (Title Search) Transfer of  
Land Act 1958**

Page 1 of 1

VOLUME 06391 FOLIO 073

Security no : 124063596485P

Produced: 02/12/2016 03:53 pm

**ADVERTISED PLAN**

**LAND DESCRIPTION**

Lots 1 and 2 on Title Plan 851207R.

PARENT TITLES :

Volume 02323 Folio 512 Volume 03817 Folio 379

Created by instrument 1765:65 14/03/1940

**4 OF 29**

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**REGISTERED PROPRIETOR**

Estate Fee Simple

Sole Proprietor

AC WELLAND MBM PTY LTD of LEVEL 9 550 BOURKE STREET MELBOURNE VIC 3000

AM304158T 05/11/2015

**ENCUMBRANCES, CAVEATS AND NOTICES**

MORTGAGE AM304159R 05/11/2015

NATIONAL AUSTRALIA BANK LTD

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**DIAGRAM LOCATION**

SEE TP851207R FOR FURTHER DETAILS AND BOUNDARIES

**ACTIVITY IN THE LAST 125 DAYS**

NIL

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

**ADMINISTRATIVE NOTICES**

NIL

eCT Control 16089P NATIONAL AUSTRALIA BANK LIMITED  
Effective from 23/10/2016

DOCUMENT END

<b>TITLE PLAN</b>	<b>EDITION 1</b>	<b>TP 851207R</b>
<b>Location of Land</b> Parish: MOORABBIN Township: - Section: - Crown Allotment: DENDY'S CROWN SPECIAL SURVEY (PT) Crown Portion: -  Last Plan Reference: LP 3079 Derived From: VOL 6391 FOL 073  Depth Limitation: -		<b>Notations</b>      ANY REFERENCE TO MAP IN THE TEXT MEANS THE DIAGRAM SHOWN ON THIS TITLE PLAN
<b>Description of Land/ Easement Information</b>  with a right to use the land colored yellow on the said map for party wall purposes - As to Lot 28 Together with a right of carriage way over the roads colored brown and yellow on the Plan of Subdivision No. 3079 - As to the aforesaid part of Lot 27 Together with a right of carriage way over the road colored brown on the said map and over Nicholson Street colored yellow and Lees Street colored brown on the Plan of Subdivision No. 3079		Together --- THIS PLAN HAS BEEN PREPARED BY LAND REGISTRY, LAND VICTORIA FOR TITLE DIAGRAM PURPOSES  COMPILED: Date 12/04/06 VERIFIED: A. DALLAS Assistant Registrar of Titles  <b>COLOUR CODE</b> BL=BLUE G=GREEN BK=BROWN P=PURPLE Y=YELLOW R=RED
<b>ENCUMBRANCES</b> As to the land colored blue --- THE PARTY WALL EASEMENT reserved by Instrument of Transfer No.1765365 in the Register Book -- <div style="text-align: center;">             M<sup>C</sup>. KINNON ROAD              E.           </div> <div style="text-align: center;">             LOT 1              LOT 2              LOT 28              ROAD              NICHOLSON STREET           </div>		
<b>TABLE OF PARCEL IDENTIFIERS</b> WARNING: Where multiple parcels are referred to or shown on the Title Plan this does not imply separately disposable parcels under Section 6A of the Sale of Land Act 1992 LOT 1 = LOT 28 BLOCK A ON LP 3079 LOT 2 = LOT 27 (PT) BLOCK A ON LP 3079		
LENGTHS ARE IN FEET AND INCHES	Metres = 0.3048 x Feet Metres = 0.201168 x Links	Sheet 1 of 1 Sheets





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**REGISTER SEARCH STATEMENT (Title Search) Transfer of  
Land Act 1958**

Page 1 of 1

VOLUME 06885 FOLIO 838

Security no : 124063596592X  
Produced 02/12/2016 03:55 pm

**LAND DESCRIPTION**

Lot 29 Block A on Plan of Subdivision 0030079  
PARENT TITLE Volume 03859 Folio 656  
Created by instrument 2015242 12/07/1946

**ADVERTISED BY**  
5 OF 29  
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and is not to be used for any other purpose  
under the Planning and Environment Act  
1987. The State of Victoria does not accept any  
responsibility for any such use.

**REGISTERED PROPRIETOR**

Estate Fee Simple

Sole Proprietor

AC WELLAND MBM PTY LTD of LEVEL 9 550 BOURKE STREET MELBOURNE VIC 3000  
AM304155A 05/11/2015

**ENCUMBRANCES, CAVEATS AND NOTICES**

MORTGAGE AM304156X 05/11/2015  
NATIONAL AUSTRALIA BANK LTD

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24 Subdivision Act 1988 and any other encumbrances shown or entered on the  
plan or imaged folio set out under DIAGRAM LOCATION below.

**DIAGRAM LOCATION**

SEE LP003079 FOR FURTHER DETAILS AND BOUNDARIES

**ACTIVITY IN THE LAST 125 DAYS**

NIL

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: 138 MCKINNON ROAD MCKINNON VIC 3204

**ADMINISTRATIVE NOTICES**

NIL

eCT Control 16089P NATIONAL AUSTRALIA BANK LIMITED  
Effective from 23/10/2016

DOCUMENT END

**LP 3079**

**EDITION 6**

PLAN MAY BE LODGED 09/10/1890

**3 SHEETS  
SHEET 1**

**COLOUR CODE**

E-1, E-4, E-5, E-6 & E-7 = BROWN  
E-2 = GREEN  
E-3 = YELLOW

**APPURTENANT EASEMENT**

AS TO LOTS 5, 28 & 29 SECTION A,  
LOTS 2, 6, 7, 12, 16, 20, 22, 37, 39, 50 to 53,  
55 & 57 SECTION B,  
LOTS 1, 6 to 10, 29, 34, 35, 37 to 39, 41,  
49 to 54 & 58 SECTION C,  
LOTS 1 to 5, 10, 31, 32, 35, 37, 51, 52,  
57 & 68 SECTION D:  
TOGETHER WITH A RIGHT OF  
CARRIAGEWAY OVER THE ROADS  
COLOURED BROWN ON THIS PLAN

AS TO LOTS 8 to 10, 15, 35 & 40 SECTION B:  
TOGETHER WITH A RIGHT OF CARRIAGEWAY  
OVER E-4

AS TO LOT 4 SECTION C:  
TOGETHER WITH A RIGHT OF CARRIAGEWAY  
OVER E-6

AS TO LOTS 5, 62 & 66 SECTION D:  
TOGETHER WITH A RIGHT OF CARRIAGEWAY  
OVER E-7.

**PLAN OF  
PART OF DENDY'S SPECIAL SURVEY  
PARISH OF MOORABBIN  
KNOWN AS M<sup>C</sup>KINNON ESTATE**

**COUNTY OF BOURKE**

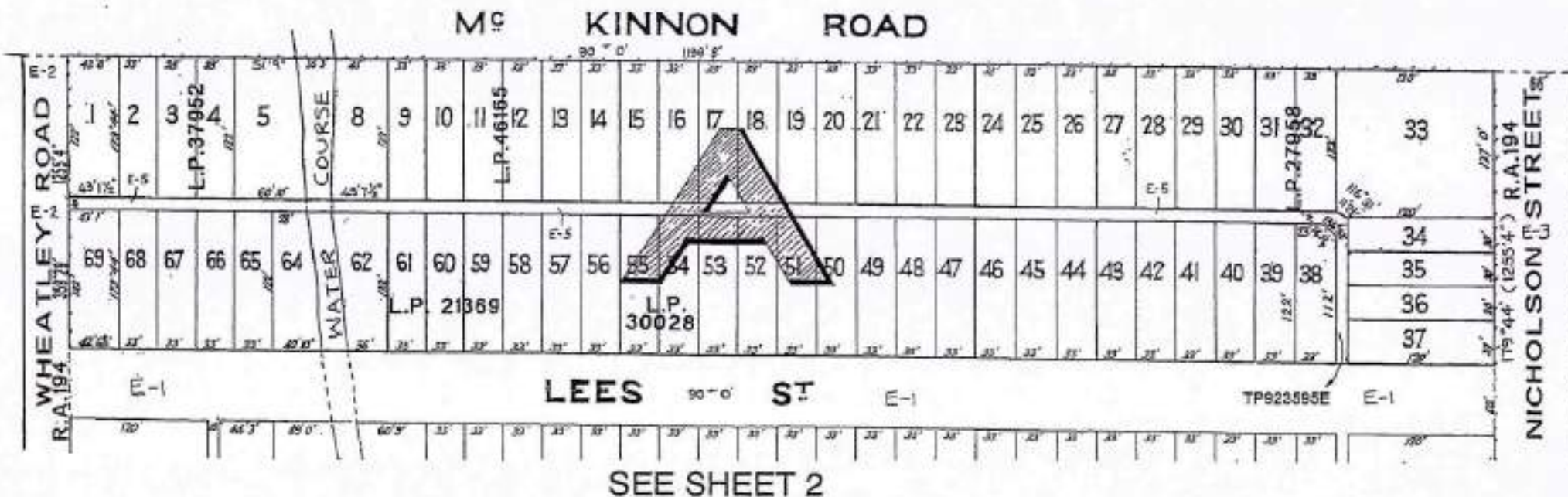
VOL.2154 FOL.639

VOL.2132 FOL.203

Measurements are in Feet & Inches

Conversion Factor

FEET X 0.3048 = METRES



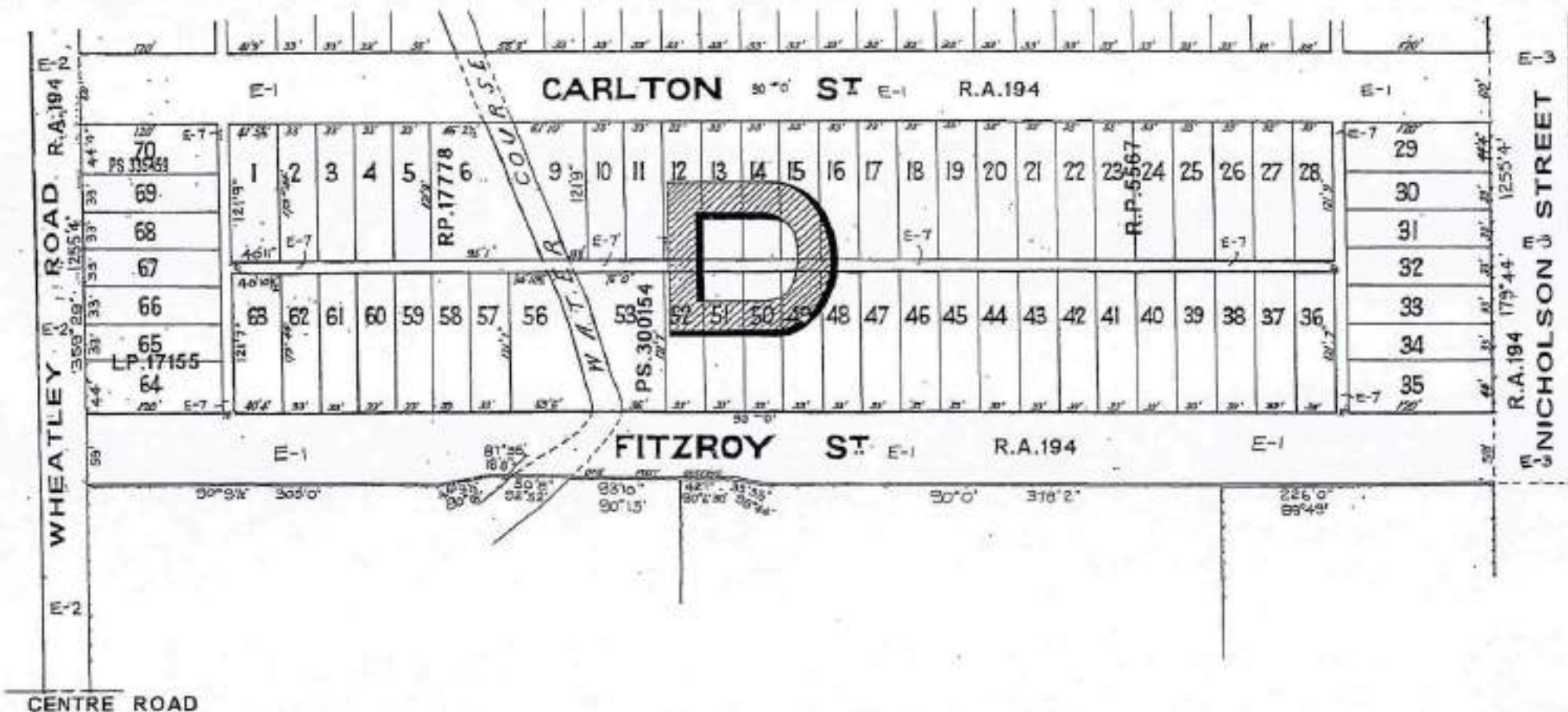




LP 3079

3 SHEETS  
SHEET 3

SEE SHEET 2





# MODIFICATION TABLE

RECORD OF ALL ADDITIONS OR CHANGES TO THE PLAN

PLAN NUMBER

LP 3079

AFFECTED LAND / PARCEL	LAND / PARCEL / IDENTIFIER CREATED	MODIFICATION	DEALING NUMBER	DATE	TIME	EDITION NUMBER	ASSISTANT REGISTRAR OF TITLES
PT. LOT 5 & PT. WATER COURSE		EXCISED	AP. 77753N SEC. 60			1	
WHEATLEY ROAD		ROAD NAME CHANGED FROM ELIZABETH STREET	CORR. 1935/ 11609			1	
THIS PLAN		APPURTENANT EASEMENTS NOTED				2	
		WARNING: THE IMAGE OF THIS PLAN/DOCUMENT HAS BEEN DIGITALLY AMENDED. NO FURTHER AMENDMENTS ARE TO BE MADE TO THE ORIGINAL PLAN/DOCUMENT.					
THIS PLAN		APPURTENANCY NOTATION ENHANCED				3	MLB
PT. LOT 6 BLOCK C & PT. WATER COURSE		EXCISED	AP125150G SEC. 60	18/02/08		4	PA
ROAD	TP920595C	REMOVAL OF EASEMENT	AP125423W SEC 73	07/03/08		5	QV
PART WATER COURSE	LOT 1 TP957647V	EXCISED	AP131138L SEC.60	16/08/15		6	BT



15 December 2016



## ADVERTISED PLAN

Statutory Planning Department

Glen Eira City Council

PO Box 42

Caulfield South VIC 3162

Att: Adam Stark



Dear Sir/Madam

**Re: Planning Permit Application: Apartment Building  
134-138 McKinnon Road, McKinnon**

We act for AC Welland McKinnon Pty Ltd. On behalf of our client we submit the accompanying Planning Permit Application for the development of the land for the purpose of a three storey residential development.

Accompanying this letter please find the following documents;

- Complete Planning Permit Application Form and Application Fee
- Metropolitan Planning Levy Certificate
- Copy of Titles
- Architectural Submission by SGA Design (3 x A3 copies)
- Urban Context Report
- Traffic Engineering Assessment by Traffix Group

Should you require any further information please contact our office on the number provided.

Yours faithfully,

Damian Loughnan  
G2 Urban Planning

Glen Eira City Council

Ref. No: Plan/20761



# PLANNING REPORT

## APARTMENT BUILDING

134-138 MCKINNON ROAD MCKINNON

DECEMBER 2016

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## 1. INTRODUCTION

G2 Urban Planning has been instructed by "AC Welland McKinnon P/L" to prepare this submission in support of a planning permit application for an apartment development at 134-138 McKinnon Road McKinnon.

The architectural drawings by SGA Architects demonstrate a modern and innovative response to this site, incorporating a built form with graduated setbacks and detailing that will enhance the streetscape. This application is also supported by a Traffic Engineering Assessment by Traffix Group.

A pre-application meeting was held with Council planning officers on October 26 2016.

This planning report expands on the diagrammatic explanation of the proposal as shown on the submitted plans, addresses the appropriate policies relating to this proposal and responds to the key issues relating to the site.

Planning Application GEPP-28176/2015 for 22 dwellings on the site was refused by Council in November 2015. The merits of the proposal were considered in an appeal to the Victorian Civil Administrative Tribunal in May 2016. VCAT subsequently directed that no permit be granted.

The VCAT Order was supportive of the concept generally, however raised several design and layout concerns and concluded that:

*I consider that this is a site that represents a substantial development opportunity within the activity centre. However, I am not satisfied that this design response is acceptable.*

*I did consider whether my concerns could be addressed through conditions given that Council provided a long list of draft conditions for discussion at the hearing. However, I consider that an integrated approach to the design changes is required in this case.*

This current application provides a highly modified design response addressing all of the planning issues raised in the VCAT determination. Significantly the current proposal has been reduced in its scale ensuring that a number of issues that were deemed satisfactory in the VCAT Order have greater suitability under this current application.

## 2. SITE CONTEXT

### 2.1 Strategic Context

The site is comprised of three lots at 134, 136 & 138 McKinnon Road, McKinnon at the edge of the McKinnon shopping centre which is a small neighbourhood shopping centre dating back to the 1930's.

A series of attached two storey 1930's era commercial terraces are on the north side of McKinnon Road between Elm Grove and the train line. Several terraces are also on the south side of the street, however to the east are recently constructed mixed use buildings with ground floor commercial space to the street as well as dwellings in the upper levels.



Figure 1: Locality Map (Source: Melway)

Residential properties to the west are interspersed with various commercial uses such as a dental practice on the adjoining 132 McKinnon Road and a Council public hall at No. 122.

To the south of the site are properties with single dwellings fronting Leeson St, a number in the process of redevelopment.



The McKinnon neighbourhood shopping centre is designated in the Glen Eira Planning Scheme as a housing diversity area and this has been reflected in a number of three and four storey buildings constructed near the site and also east of the train line.

Other than the original heritage buildings that are protected under a heritage overlay, this centre will continue to evolve as an area of multi-storey buildings.

The site is well positioned being 150m from the McKinnon train station which is on the Frankston line while the route 626 bus on McKinnon Road links Middle Brighton Station and the Chadstone Shopping Centre.



Figure 2: Aerial of the locality (Source: Nearmap)

## 2.2 Site Context

The site has an area of approximately 1125m<sup>2</sup> with a frontage to McKinnon Road of approximately 30m and depth of 37.5, adjoining a lane to the rear which provides direct access with Nicholson Street which is 100m to the east.

The site is formally described as: Lot 1 TP 571838N; Lots 1 & 2 TP 851207R and; Lot 29 Block A PS 003079.

The three lots comprise of a 1950's era brick veneer duplex on 134 and 136 and a weatherboard interwar dwelling at No. 138, each of these lots having a crossover to the street.

The dwellings have a similar front setback and a rear yard each with a shed and limited landscaping.

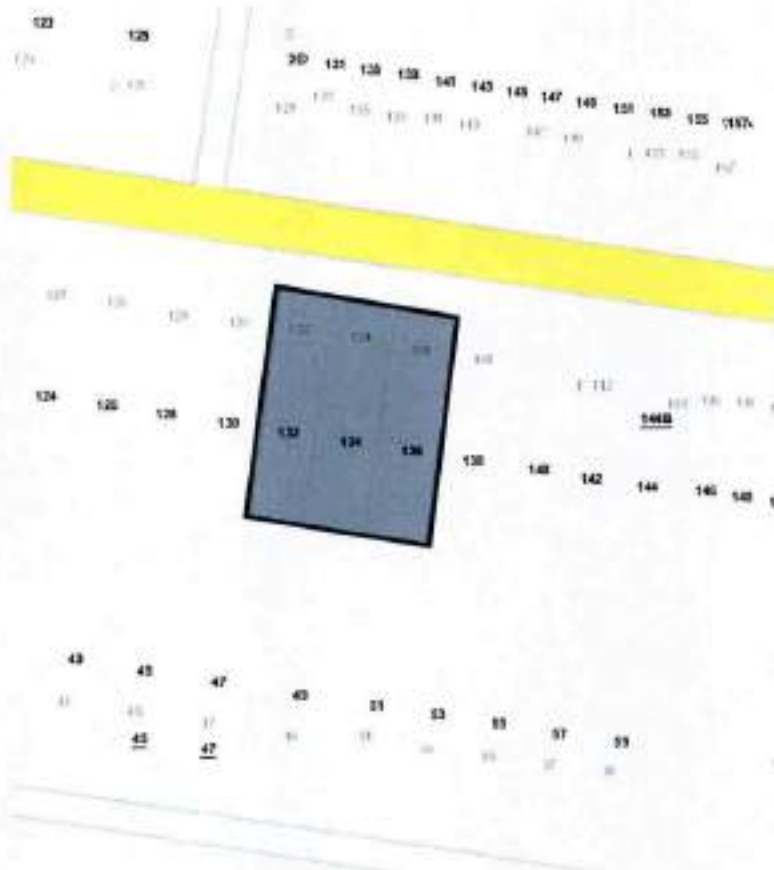


Figure 3: Cadastral plan of the locality (Source: Lassi)



site frontage





view toward the site, looking east



140-144 McKinnon Road



McKinnon Road activity centre looking east



rear access of 140 McKinnon Rd



rear car park of dental practice at 132 McKinnon Rd



rear walls of apartments to the east





dwelling to the south of the site

Adjacent land to the east comprises a number of recently constructed buildings of grey, white and similar tones summarised as follows:

140 McKinnon Rd	Planning Permit 15413/2003 provided for the three storey building of 8 dwellings and a café. This building presents a robust form of grey panels that face the site
142 McKinnon Rd	Planning Permit 194/4.2007B provided for the three storey building of 4 dwellings and a shop
144 McKinnon Rd	Four storey building of 14 dwellings
156 McKinnon Rd	Planning permit 17372/2005/A provided for the three storey building of four dwellings and a shop (this site is on the corner of Nicholson St)
95 Nicholson St	Planning permit 23132/2001 provided for the three storey building of 26 dwellings (this site also fronts Lees St)

Land adjoining to the west at No 134 McKinnon Road comprises a dental practice in a two storey building which is brick with a charcoal tiled roof, having the appearance of a dwelling. At the rear of the site is a paved parking area which is accessed from the rear lane. A No. 130 is a weatherboard interwar dwelling with a gable end to the street and centrally pitched ridge ridge. There are otherwise single dwellings on lots further west as well as the municipal hall at No. 122.

A row of attached 1930's era terraces are on the north side of McKinnon Road. There is also a two storey infill building at No. 125 and three storey buildings at 119-121 McKinnon Road and 2 Elm Grove. The McKinnon Memorial Gardens, a local park is at the corner of McKinnon Road and Wattle Grove.

Single dwellings on the Lees St residential properties to the south have their private open space facing north which is buffered from the site by the intervening lane.

### 3. THE PROPOSAL

#### 3.1 Overview

The Architectural Drawings by SGA Design present a three storey building on this site comprising of 21 dwellings and a basement with 26 car spaces. The dwelling breakdown is:

1 x 1 bedroom; 18 x 2 bedroom and; 2 x 3 bedroom.

The building has a maximum height of 11.12m at the southern part of the site where the site falls away.

A basement car park is accessed from the rear lane. The development will remove the current crossovers to McKinnon Road and replace them with three on-street parking spaces.

There is good provision for landscaping at ground level with landscape strips up to 3m wide.

The car parking spaces will be allocated as follows:

- 24 car stacker spaces each 1 and 2 bedroom dwelling will be allocated one car space and the 2 x 3 bedroom dwellings will each have two spaces
- 2 non-stacker visitor spaces and one visitor space as a stacker (alternatively it may be allocated as an additional space for a dwelling)

There are dedicated storage cages for each dwelling and 21 bicycle spaces in the basement as well as at the frontage. A bin room for refuse collection and plant room in the basement. A substation is defined at the rear of the site accessible by the adjacent lane.

The building façade perspective view is included in the application plans at TP21. The building has minimum floor to ceiling heights of 2.55m and is "capped" by a Colorbond pitched roof. The materials include face brick at ground floor and part of the first floor the south-east corner. The building otherwise features timber-look, alucobond and Corten cladding. The front fence includes a rendered masonry wall with metal palisades to a height of 1.5m.

A comparison between the former VCAT and the current proposal is outlined in Table 1.

Table 1: Comparison of former VCAT Plans and December 2016 Plans

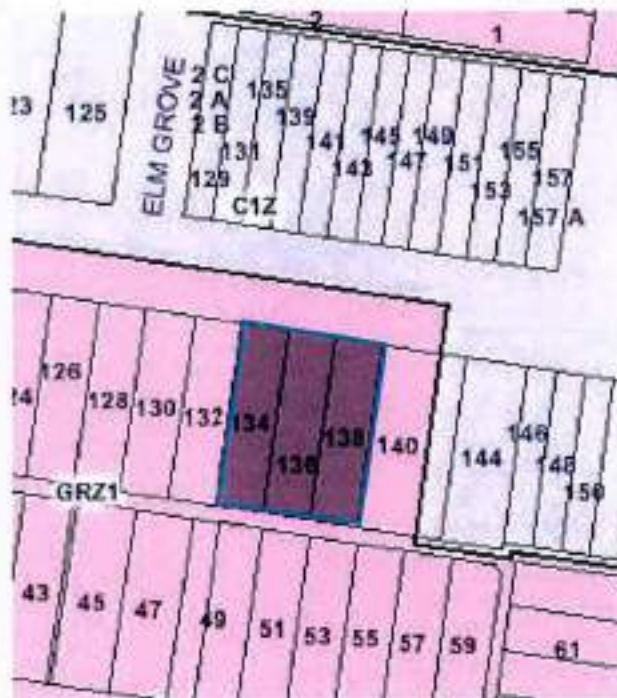
BUILDING ELEMENT	VCAT PLANS: MAY 2015	NEW PROPOSAL: DEC 2016
dwellings	22	21 Dwellings are now smaller and there are fewer bedrooms overall
car spaces	27 with no visitor spaces	26 with 2 visitor spaces (and a third as a car stacker or allocated to a dwelling)  (due to the reduction of 1 dwelling and removal of 2 x 3 bedroom dwellings the overall number of required spaces is less)
Basement Level		The basement is reduced in size; its setbacks are increased to all site boundaries. It is 2.6m to 4m from the boundaries
Ground floor		
Front setback	Minimum 1.8m	Minimum 3.1m
Open space/courtyard	Minimum 2m wide	All at least 3.1m wide
First floor		
Front setback	Minimum 1.8m	4.66m
Second floor		
Front setback	4.2m	5.5m to 6m
Dwelling floorspace		The dwellings have an overall reduction in floorspace of approximately 44m <sup>2</sup>
Building site coverage	74.5%	67.6%
Site permeable area	18.5%	22.9%
Height	11.14m	11.12m. There is a reduction in the floor levels to a minimum 2.55m and reduction of space between each floors, which allows for the provision of a pitched roof
Elevations		Building elevations have increased articulation and material and colour variation. The provision of a low pitched roof enhances the appearance from a distance



## GLEN EIRA PLANNING SCHEME CONTROLS

### 3.2 Zoning

The site is zoned General Residential Zone Schedule 1. The purpose of the Zone encourages development that respects neighbourhood character, provides housing diversity and moderate growth.



One of the Clause 4 Decision guidelines refers to "Opportunities to maximise development through consolidation of lots" which will occur through this proposal.

### 3.3 Overlays

#### Parking Overlay Schedule PO2

The site is included in the Parking Overlay which identifies areas and uses where local parking rates apply. This site is within an area where reduced rates apply to student housing. This application does not propose student housing and so parking requirements are otherwise specified under Clause 52.06 of the Planning Scheme.

### 3.4 Clause 52.06 – Car Parking

Clause 52.06 specifies vehicle parking and access requirements. One car space is required for 1 and 2 bedroom dwellings and 2 car spaces are required for 3 or more bedroom dwellings. One visitor car space is required per 5 dwellings. The development requires 27 spaces and as 26 have been provided, a permit is required for a reduction of one car parking space (note one of these spaces is in the form of a stacker and so it would be preferable for it to be allocated as an additional dwelling space).

### 3.5 State Planning Policy

The State Planning Policy Framework (SPPF) outlines guiding parameters for urban consolidation, business development and community accessibility. The applicable Clauses of the SPPF and relevant objectives are as follows:

#### Clause 9 - Plan Melbourne

Plan Melbourne encourages more liveable environments and design excellence in residential areas.

#### Clause 15.02-1 "Energy and resource efficiency"

This Clause encourages land use and development that is consistent with the efficient use of energy and the minimisation of greenhouse gas emissions.

#### Clause 15.01-2 - Urban Design Principles

This Clause seeks to achieve architectural and urban design outcomes that contribute positively to local urban character and enhance the public realm, while minimising detrimental impact on neighbouring properties.



## Clause 16 "Housing"

*Planning should provide for housing diversity, and ensure the efficient provision of supporting infrastructure.*

*New housing should have access to services and be planned for long term sustainability, including walk ability to activity centres, public transport, schools and open space.*

*Planning for housing should include providing land for affordable housing.*

Clause 16.01-3 Strategic redevelopment sites Objective is:

- *To identify strategic redevelopment sites for large residential development in Metropolitan Melbourne.*

Strategic redevelopment sites as applicable to this site are:

- *In or beside Neighbourhood Activity Centres that are served by public transport.*
- *On or abutting tram, train, light rail and bus routes that are part of the Principal Public Transport Network and close to employment corridors, Central Activities Districts, Principal or Major Activity Centres.*
- *In or near major modal public transport interchanges that are not in Principal or Major Activity Centres.*
- *Able to provide 10 or more dwelling units, close to activity centres and well served by*
- *public transport.*

## 3.6 Local Planning Policy

### Clause 21 - Municipal Strategic Statement

#### Clause 21.03 – Vision Strategic Framework

The Municipal Strategic Statement provides planning and development directions around a number of land use themes. The theme that is directly applicable to this application is "Housing and Residential Development".

A Framework Plan of the municipality identifies the site as being within the McKinnon Neighbourhood Centre. They are described as appropriate for the following development:

*Apartments and shop top housing are encouraged within the commercial areas of these centres. Single dwellings and multi-unit development are encouraged immediately adjoining the commercial areas of these centres.*

#### Clause 21.04 – Housing and Residential Development

The Planning Scheme refers to the need for the municipality to accommodate housing growth into the future and the need to address changing demographics such as the "empty nesters" market:

*Providing a wider range of housing types will address the changing housing needs of Glen Eira's existing population (eg empty nesters seeking smaller homes, people beginning families, older people seeking retirement housing).*

This Clause also states that:

*There are numerous environmental, social and economic benefits in locating multi-unit development close to public transport and services. By locating multi-unit development close to public transport and other facilities there is a reduced reliance on the car, which in turn reduces emissions, congestion and parking issues.*

Encouraging a mix of housing types, increased residential densities and mixed use developments in urban villages and neighbourhood centres will help to stimulate and revitalise Glen Eira's shopping centres. Residential development in these locations should help create safer, more attractive and lively community focuses.

This policy includes objectives for residential development, as relevant to this site are the following:

- *To ensure a greater diversity of housing to meet future housing needs;*
- *To improve and protect the liveability, neighbourhood character and amenity of Glen Eira and;*
- *To stimulate and improve the vitality of Glen Eira's commercial centres.*

Strategies include:

- *Guide multi-unit development towards obvious strategic locations that have good access to public transport, commercial, community, educational and recreational services and facilities*
- *Ensure new residential development enhances the character of the neighbourhood.*
- *Facilitate high quality urban design and architecture that will enhance neighbourhood character.*
- *Encourage a mix of housing types, increased residential densities and mixed use developments within urban villages and neighbourhood centres.*

#### Clause 22.07 Housing Diversity Area Policy

This policy supports development in specific areas designated for increased density such as the McKinnon Neighbourhood Centre which is in the Commercial 1 Zone and adjacent land is in the General Residential Zone which encourages apartment development up to 10.5m high (or 11.5m where the slope is greater than 2.5 degrees). McKinnon is one of 10 neighbourhood centres which include the commercial areas and a surrounding residential area.



The broad objectives under Clause 22.07-2 refers to the need for housing diversity, a mix of dwelling sizes, and to ensure that development takes into account its interface with residential development on adjacent sites.

For neighbourhood centres it is policy to:

- *Recognise neighbourhood centres as locations which provide significant opportunities for housing diversity, but at a lesser scale and density than developments in urban villages and the Phoenix Precinct.*
- *Recognise that different development outcomes are sought in the commercial and residential areas of neighbourhood centres.*

For residentially zoned land in neighbourhood centres, including the development site it is policy to:

- *Encourage a mix of dwelling types and layouts.*
- *Recognise that these areas offer opportunities for multi-unit development, but at a lower scale and density than development in the commercial and mixed use areas of neighbourhood centres.*
- *Ensure that the density, mass and scale of residential development is appropriate to that of the neighbourhood centre.*
- *Ensure that residential development is sited and designed so that it does not dominate the streetscape.*
- *Encourage the consolidation of sites to promote development opportunities.*
- *Encourage a decrease in the density of residential development as the proximity to the commercial area of the neighbourhood centre decreases.*
- *Ensure that the siting and design of residential development responds positively to its interface with existing residential development in minimal change areas.*

## 4. PLANNING CONSIDERATIONS

### 4.1 Policy Overview

The Glen Eira Planning Scheme identifies most residential land in the municipality as being within a "Minimal Change Area" and so restricted to only two dwellings per lot. The remaining areas are either larger localities for development being the Bentleigh, Carnegie and Elsternwick Urban Villages or the Phoenix Precinct around the Caulfield Racecourse, Railway Station and Monash University. The ten urban villages including McKinnon have a secondary status to these localities.

Land in the Commercial 1 Zone to the east and north has no height restrictions, any proposal requiring an assessment of its contextual suitability. There are a four storey and three storey buildings in the Commercial 1 Zoned land to the east.

A three storey apartment building for 26 dwellings was constructed in early 2016 at 95 Nicholson St, on the corner with Lees St. Planning Permit 23132/2010 was issued for the site at the direction of VCAT in 2011 (VCAT reference: P1135/2011).

The Tribunal referred to that site having the following policy attributes which is also consistent with this site.

*The important features of the site, insofar as relevant to policy, are:*

- *It is in a Neighbourhood Activity Centre (NAC) and adjoining the commercial strip.*
- *It is a Strategic Redevelopment Site (SRS), being a site where it is possible to achieve the objective to*  
*...locate new housing in or close to activity centres and employment corridors and at other strategic redevelopment sites that offer good access to services and transport.*  
*and is*  
*able to provide 10 or more dwelling units, close to activity centres and well served by public transport.*
- *It is close to a railway station on the Principle Public Transport Network.*
- *It is within the Planning Scheme's Housing Diversity Area (HDA). (para 15)*

*The policies indicate that development of the greatest scale within an HDA should be on sites in the commercial areas, stepping down in scale to sites outside these commercial areas and down again to residential areas outside the HDA (these invariably being minimal change areas. (para 17)*

*The policies require that housing development of greater scale or variety should nevertheless be appropriate to its context and that buildings of greater height than their neighbours should step down appropriately to the latter. (para 18)*



## 4.2 The Key Planning Issues

The key planning issues relate to the building presence, setbacks to the front and side boundaries, landscape opportunities and internal amenity borne out in the VCAT Order for the previous planning permit application. The development has now been modified to address these key issues as outlined below.

### Height and Streetscape

The VCAT Order commented on the frontage treatment as follows:

*However, I agree with Council that the proposal's site layout and massing has not been acceptably resolved. The proposal has positioned the entrance at the north east corner, abutting the pedestrian entry for the apartments to the east. A minimal setback of 1.85m at ground floor and to first floor balconies is then provided for the majority of the frontage, stepping back to 3.7m at the northwest corner of the land. (para 15)*

*While I don't find that a significant setback to the third level (such as the 6m suggested in the Council's draft conditions) is required to the upper floor, the overall building design needs to be broken up more effectively across the frontage. (para 17)*

In response the plans now have:

- a minimum front setback of 3.1m The setback exceeds the Planning Scheme Clause 55 Standard B6 setback of 2.63m
- centrally located pedestrian entrance
- recessive upper levels with the second floor setback 5.5m to 8.7m
- reduced basement with extensive area for landscaping at all interfaces
- A facade that has a recessed break and a separate projecting timber look feature at first and corten cladding at second floor, all which provide negative and positive spaces and segment the horizontality of the facade
- A more conventional residential appearance through a hipped roof
- The building reflects a more integrated design and has a consistent architectural statement

### East and west interfaces

The VCAT Order considered the east and west interfaces as follows:

*To the west elevation the proposal presents extensive building form which does not comply with Standard B17 of Clause 55. The agreement with the neighbour has removed a level of articulation to its detriment. (para 19)*

*While there is a separation of built form to the east (partly in response to the apartments that have outlook to the subject site), the proposal does not comply with Standard B17 for significant sections of the building and balconies. I find that this is not acceptable to the rear of the site (para 20)*

In response:

- The setbacks all now exceed Standard B17 of Clause 55 of the Planning Scheme
- The setbacks are largely 3m at ground level
- The first floor western minimum setback is increased from 2.2m to 2.6m
- The setback to the east is reduced due to the re-located pedestrian access, however are significant and in excess of Standard B17
- The ground floor east wall toward the rear in the former plans now has an increased setback from a minimum 1m to 3m (see Unit 105)

### **Southern Interface**

The VCAT Order advised that:

*While there must be an expectation of change to outlook for these properties, I do agree with Council that the design detail including the length of the upper level balconies unduly emphasises the width of this development and is not well resolved. (para 21)*

The southern elevation is now simplified:

- there are 3m side setbacks to each side boundary at first floor
- the balconies at the second floor are separated and reduced in extent and there is also greater recession to each side boundary

### **Internal Amenity**

The VCAT Order discussed internal amenity as follows:

*The Council raised a number of concerns about internal amenity including the size and orientation of open spaces. The officer's report also raised some concern about the internal layout of apartments and daylight access. A number of bedrooms are provided with daylight from a central lightwell/ courtyard area only. (para 27)*

The internal amenity is addressed as follows:

- The central lightcourt is now substantial in size, increased in width from 2m to 3.8m
- The internal layout is revised: there are no "saddleback" bedrooms and no borrowed light rooms



- All dwellings have good access to daylight: the previous overhangs being removed
- Unit G.06 includes one bedroom facing the passing bay/lane, and a window inset behind a 600mm wide landscaped planter box.

#### Traffic and Car Parking

Traffic and car parking was deemed appropriate, the VCAT Order stating that:

*The proposal provides the required resident car parking under Clause 52.06 on site and this matter is not before me. A reduction in four visitor car spaces is sought by this proposal. Council's traffic engineers did not object subject to the provision of two visitor car spaces on site. (para 30)*

#### 4.3 Building Height

The Planning scheme seeks to limit buildings to 3 storeys. The General Residential Zone Schedule 1 states that:

*A building used as a dwelling or residential building must not exceed a height of 10.5 metres. (where the slope of the natural ground level at any cross section wider than 8 metres of the site of the building is 2.5 degrees or more, it must not exceed a height of 11.5 metres. A lift overrun may not exceed the mandatory height by more than 5m. Based upon an assessment that the slope is more than 2.5 degrees a four storey building could be proposed. However it would be constrained by the maximum height of 11.5m requiring low floor to ceiling heights, and would provide an inferior product than a three storey development.*

The site has a slope of 12 degrees and therefore a height up to 11.12m is consistent with the zone.



frontage interface with 140 McKinnon Rd



frontage interface with 132 McKinnon Rd

#### 4.4 Glen Eira Planning Scheme Clause 55 Assessment

The proposed development is assessed under the Standards and Objectives of Clause 55 of the Glen Eira Planning Scheme.

##### Clause 55 Assessment

Objectives	Standard	Compliance
<p><b>Clause 55.02-1</b></p> <p><b>Neighbourhood Character Objectives:</b></p> <p>To ensure that the design respects the existing neighbourhood character or contributes to a preferred neighbourhood character;</p> <p>To ensure the development responds to the features of the site and surrounding area.</p>	<p><b>Standard B1</b></p> <p>The design response <u>must</u> be appropriate to the neighbourhood and the site.</p> <p>The proposed design must respect the existing or preferred neighbourhood character and respond to the features of the site.</p>	<p>The character of the area is comprised of pre-war and interwar dwellings with sites of infill development over recent decades and contemporary apartment. There are generally spaces between buildings, scattered landscaping, and a mix of height and otherwise predominantly single storey dwellings to the south and east.</p> <p>The proposed building has good setbacks from all site boundaries.</p> <p>Complies</p>
<p><b>Clause 55.02-2</b></p> <p><b>Residential Policy Objectives</b></p> <p>To ensure that residential development is provided in accordance with any policy for housing in the SPPF and the LPPF including the MSS and local planning policies;</p> <p>To support medium densities in areas where development can take advantage of public transport and community infrastructure and services.</p>	<p><b>Standard B2</b></p> <p>An application <u>must</u> be accompanied by a written statement that describes how the development is consistent with any relevant policy for housing in:</p> <ul style="list-style-type: none"> <li>the SPPF; and</li> <li>the LPPF including the MSE; and Local Planning Policies</li> </ul>	<p>State Planning Scheme Policy influences are:</p> <p>Clause 15.01-1 Urban Design</p> <p>Clause 16 Housing</p> <p>Local Planning Scheme Policy influences are:</p> <p>Clause 21.04 – Housing and Residential Development</p> <p>Clause 22.07 – Housing Diversity Area Policy</p> <p>The development is in compliance with policy and provides appropriate medium density housing.</p>
<p><b>Clause 55.02-3</b></p>	<p><b>Standard B3</b></p>	<p>The development includes a range of dwellings types and sizes.</p>



<b>Dwelling Diversity Objective</b>  To encourage a range of dwelling sizes and types in development of ten or more dwellings	Developments of 10 or more dwellings <u>should</u> provide a range of dwelling sizes and types including: <ul style="list-style-type: none"> <li>• dwellings with a different number of bedrooms; and</li> <li>• at least one dwelling with a kitchen, bath or shower, and toilet and wash basin at ground floor level.</li> </ul>	Complies
<b>Clause 55.02-4</b>  <b>Infrastructure Objectives</b>  To ensure development is provided with appropriate utility services and infrastructure;  To ensure development does not unreasonably overload the capacity of utility services and infrastructure.	<b>Standard B4</b>  Development <u>should</u> be connected to reticulated services including reticulated sewerage, drainage, electricity and gas if available.  Developments <u>should</u> not unreasonably exceed the capacity of utility services and infrastructure, including reticulated services and roads.  In areas where utility services or infrastructure have little or no space capacity, developments <u>should</u> provide for the upgrading or mitigation of the impact on services or infrastructure.	Complies
<b>Clause 55.02-5</b>  <b>Integration with the Street Objective</b>  To integrate the layout of development with the street	<b>Standard B5</b>  Developments <u>should</u> provide adequate vehicle and pedestrian links that maintain or enhance local accessibility.  Dwellings <u>should</u> be orientated to front existing and proposed streets  High fencing in front of dwellings <u>should</u> be avoided if practicable.  Development next to existing public open space <u>should</u> be laid out to complement the open space.	A 1.5m high masonry and metal palisade fence fronts McKinnon Rd Complies: the upper part of the fence provides transparency
<b>Clause 55.03-1</b>  <b>Street Setback Objective</b>  To ensure that the setbacks of buildings from a street respect the existing or preferred neighbourhood character and make efficient use of the site.	<b>Standard B6</b>  Walls of buildings <u>should</u> be setback from streets the distance specified in Table B1	The front setback required would be 2.63m an average of 5.26m at No 132 and zero at No. 140 McKinnon Rd Complies
<b>Clause 55.03-2 Building Height Objective</b>  To ensure that the height of buildings respects the existing or preferred neighbourhood character.	<b>Standard B7</b>  The maximum building height should not exceed the maximum height specified in the zone, schedule to the zone or an overlay that applies to the land.  If no maximum height is specified in the zone, schedule to the zone or an overlay, the maximum building height should not exceed 9 metres, unless the slope of the natural ground level at any cross section wider than 8 metres of the site of the	The maximum height at 11.12m is consistent with the Zone which specifies up to 11.5m for this sloping site. Complies

	<p>building is 2.5 degrees or more, in which case the maximum building height should not exceed 10 metres.</p> <p>Changes of building height between existing buildings and new buildings should be graduated</p>	
<p><b>Clause 55.03-3 Site Coverage Objective</b></p> <p>To ensure that the site coverage respects the existing or preferred neighbourhood character and responds to the features of the site</p>	<p><b>Standard 88</b></p> <p>The site area covered by buildings <u>should</u> not exceed 60%.</p>	<p>Site coverage is 67.6%.</p> <p>Does not comply with the standard.</p> <p>Complies with the objective</p>
<p><b>Clause 55.03-4</b></p> <p><b>Permeability Objectives</b></p> <p>To reduce the impact of increased stormwater run-off on the drainage system;</p> <p>To facilitate on-site stormwater infiltration.</p>	<p><b>Standard 89</b></p> <p>At least 20 % of the site <u>should not</u> be covered by impervious surfaces.</p>	<p>Permeable area is greater than 20% being 22.9%</p> <p>Complies</p>
<p><b>Clause 55.03-5</b></p> <p><b>Energy Efficiency Objectives</b></p> <p>To achieve and protect energy efficient dwellings and residential buildings;</p> <p>To ensure the orientation and layout of development reduce fossil fuel energy use and make appropriate use of daylight and solar energy</p>	<p><b>Standard 810</b></p> <p>Buildings <u>should</u> be:</p> <ul style="list-style-type: none"> <li>• Orientated to make appropriate use of solar energy.</li> <li>• Sited and designed to ensure that the energy efficiency of existing dwellings on adjoining lots is not unreasonably reduced.</li> <li>• Living areas and private open space <u>should</u> be located on the north side of the dwelling, if practicable. Developments <u>should</u> be designed so that solar access to north-facing windows is maximised.</li> </ul>	<p>East, west and northern solar access is maximised, and there are only one two south facing dwellings.</p> <p>Complies</p>
<p><b>Clause 55.03-6</b></p> <p><b>Open Space Objective</b></p> <p>To integrate the layout of the development with any public or communal open space provided in or adjacent to the development</p>	<p><b>Standard 811</b></p> <p>Any public or communal open space <u>should</u>:</p> <ul style="list-style-type: none"> <li>• be substantially fronted by dwellings, where appropriate;</li> <li>• provide outlook for as many dwellings as practicable</li> <li>• be designed to protect any natural features on the site; and</li> <li>• be accessible and useable.</li> </ul>	<p>Complies</p>
<p><b>Clause 55.03-7</b></p> <p><b>Safety Objective</b></p> <p>To ensure the layout of development provides for the safety and security of residents and property</p>	<p><b>Standard 812</b></p> <p>Intrances to dwellings and residential buildings should not be obscured or isolated from the street and internal accessways.</p> <p>Planting which creates unsafe spaces along streets and accessways <u>should</u> be avoided.</p> <p>Developments <u>should</u> be designed to provide good lighting, visibility and surveillance of car parks and internal</p>	<p>The layout is clear and legible.</p> <p>Complies</p>



	<p>accessways.</p> <p>Private spaces within developments <u>should</u> be protected from inappropriate use as public thoroughfares.</p>	
<p><b>Clause 55.03-8</b></p> <p><b>Landscaping Objectives</b></p> <p>To encourage development that respects the landscape character of the neighbourhood;</p> <p>To encourage development that maintains and enhances habitat for plants and animals in locations of habitat importance;</p> <p>To provide appropriate landscaping;</p> <p>To encourage the retention of mature vegetation on the site</p>	<p><b>Standard B13</b></p> <p>Landscape layout and design <u>should</u>:</p> <ul style="list-style-type: none"> <li>• Protect any predominant landscape features of the neighbourhood.</li> <li>• Take into account the soil type and drainage patterns of the site.</li> <li>• Allow for intended vegetation growth and structural protection of buildings.</li> <li>• In locations of habitat importance, maintain existing habitat and provide for new habitat for plants and animals.</li> <li>• Provide a safe, attractive and functional environment for residents.</li> <li>• Developments <u>should</u> provide for the retention or planting of trees, where these are part of the character of the neighbourhood.</li> <li>• Development <u>should</u> provide for the replacement of any significant trees that have been removed in the 12 months prior to the application being made.</li> <li>• The landscape design <u>should</u> specify landscape themes, vegetation (location and species), paving and lighting.</li> </ul>	<p>A landscape plan will be prepared prior to development.</p> <p>There is significant capacity for trees and shrubs to all site boundaries.</p> <p>Complies</p>
<p><b>Clause 55.03-9</b></p> <p><b>Access Objectives</b></p> <p>To ensure vehicle access to and from a development is safe, manageable and convenient;</p> <p>To ensure the number and design of vehicle crossovers respects the neighbourhood character</p>	<p><b>Standard B14</b></p> <ul style="list-style-type: none"> <li>• The width of accessways or car spaces <u>should</u> not exceed:             <ul style="list-style-type: none"> <li>○ 33% of the street frontage if the width of the street frontage is more than 20m; or</li> <li>○ 40% of the street frontage if the width of the street frontage is less than 20m.</li> </ul> </li> </ul> <p>No more than one single-width crossover should be provided for each dwelling fronting a street.</p> <p>The location of crossovers should maximise the retention of on-street car parking spaces.</p> <p>The number of access points to a road in a Road Zone should be minimised.</p> <p>Developments must provide for access for service, emergency and delivery vehicles.</p>	<p>Access is clear and distinct utilising the rear lane with provision of a passing bay.</p> <p>Complies</p>
<p><b>Clause 55.03-10</b></p> <p><b>Parking Location Objectives</b></p>	<p><b>Summary of Standard B15</b></p> <p>Car parking facilities <u>should</u>:</p>	<p>Parking is clear and convenient with good manoeuvring space.</p> <p>Complies</p>

<p>To provide for convenient parking for residents and visitor vehicles;</p> <p>To avoid parking and traffic difficulties in the development and the neighbourhood;</p> <p>To protect residents from vehicular noise within developments</p>	<ul style="list-style-type: none"> <li>• Be reasonably close and convenient to dwellings and residential buildings;</li> <li>• Be secure;</li> <li>• Be well ventilated if enclosed.</li> <li>• Large parking areas <u>should</u> be broken up with trees, buildings or different surface treatments.</li> <li>• Shared accessways or car parks of other dwellings and residential buildings <u>should</u> be located at least 1.5m from habitable room windows. This setback may be reduced to 1m where there is a fence at least 1.5m high or where window sills are at least 1.4m above the accessway.</li> </ul>	
<p>Clause 55.04-1</p> <p><b>Side and Rear Setbacks Objective</b></p> <p>To ensure that the height and setback of a building from a boundary respects the existing or preferred neighbourhood character and limits the impact on the amenity of existing dwellings</p>	<p><b>Summary of Standard B17</b></p> <p>New building not on, or within 150mm of boundary <u>should</u> be setback from side or rear boundaries:</p> <ul style="list-style-type: none"> <li>• 1m, plus 0.3m for every metre height over 3.6m up to 6.9m, plus 1m for every metre height over 6.9m.</li> <li>• Sunblinds, verandahs, porches, eaves, gutters etc may encroach not more than 0.5m into the setbacks of this standard.</li> </ul>	<p>The development exceeds the Standard. Complies</p>
<p>Clause 55.04-2</p> <p><b>Walls on Boundaries Objective</b></p> <p>To ensure that the location, length and height of a wall on a boundary respects the existing or preferred neighbourhood character and limits the impact on the amenity of existing dwellings</p>	<p><b>Standard B18</b></p> <p>New wall on or within 200mm of a side or rear boundary of a lot, or a carport on or within 1m of a side or rear boundary <u>should</u> not abut the boundary for a length of more than:</p> <ul style="list-style-type: none"> <li>• 10m plus 25% of the remaining length of the boundary of an adjoining lot; or the length of an existing or simultaneously constructed wall or carport whichever is the greater.</li> <li>• A new wall or carport may fully abut a side or rear boundary where the slope and retaining walls would result in the effective height of the wall or carport being less than 2m on the abutting property boundary.</li> <li>• New walls on or within 200mm of a side or rear boundary of a lot, or a carport on or within 1m of a side or rear boundary <u>should</u> not exceed an average of 3.2m height, with no part higher than 3.6m, unless abutting a higher existing or simultaneously constructed wall.</li> </ul>	<p>The only wall on boundary is to the rear where the substation is located. Complies</p>
<p>Clause 55.04-3</p> <p><b>Daylight to existing windows objective</b></p> <p>To allow adequate daylight into existing habitable room windows.</p>	<p><b>Standard B19</b></p> <p>Buildings opposite an existing habitable room window <u>should</u> provide for a light court to the existing window, of at least 3m<sup>2</sup> and 1m clear to the sky. The area may include land on the abutting lot.</p> <p>Walls or carports more than 3m high opposite an existing habitable room</p>	<p>Complies</p>



	<p>window <u>should</u> be setback from the window at least 50% of the height of the new wall if the wall is within a 55 degree arc from the centre of the existing window. The arc may be swung to within 35 degrees of the plane of the wall containing the existing window.</p> <p><b>Note:</b> Where the existing window is above ground level, the wall height is measured from the floor level of the room containing the window.</p>	
<p><b>Clause 55.04-4</b></p> <p><b>North-facing windows objective</b></p> <p>To allow adequate solar access to existing north-facing habitable room windows.</p>	<p><b>Standard B20</b></p> <p>If a north-facing habitable room window of an existing dwelling is within 3m of a boundary of an abutting lot, a building <u>should</u> be setback:</p> <ul style="list-style-type: none"> <li>1m, plus 0.6m for every metre height over 3.6m up to 6.9m, plus 1m for every metre height over 6.9m, for a distance of 3m from the edge of each side of the window.</li> </ul>	Not Applicable
<p><b>Clause 55.04-5</b></p> <p><b>Overshadowing open space objective</b></p> <p>To ensure buildings do not significantly overshadow existing secluded private open space.</p>	<p><b>Standard B21</b></p> <p>Where sunlight to the secluded private open space of an existing dwelling is reduced, at least 75%, or 40m<sup>2</sup> with a minimum dimension of 3m, whichever is the lesser area, or the secluded open space <u>should</u> receive a minimum of 5 hours sunlight between 9am and 3pm at 22 September.</p> <p>If existing sunlight to the secluded private open space of a dwelling is less than the requirements of this standard, the amount of sunlight <u>should</u> not be further reduced.</p>	Refer to shadow plans. Complies
<p><b>Clause 55.04-6</b></p> <p><b>Overlooking objective</b></p> <p>To limit views into existing secluded private open space and habitable room windows.</p>	<p><b>Standard B22</b></p> <p>Habitable room windows, balconies, terraces etc <u>should</u> be located and designed to avoid direct view to secluded private open space and habitable room windows of an existing dwelling within 9m distance, and a 45 degree arc from the window, balcony etc.</p>	Windows and balconies are all screened where there is potential for overlooking Complies
<p><b>Clause 55.04-7</b></p> <p><b>Internal Views Objective</b></p> <p>To limit views into the secluded private open space and habitable room windows of dwellings and residential buildings with a development</p>	<p><b>Standard B23</b></p> <p>Windows and balconies <u>should</u> be designed to prevent overlooking of more than 50% of the secluded private open space of a lower-level dwelling or residential building directly below and in the same development.</p>	There is no adverse overlooking Complies
<b>Clause 55.04-8</b>	<b>Standard B24</b>	Complies

<b>Noise Impacts Objectives</b>  To contain noise sources in developments that may affect existing dwellings;  To protect residents from external noise	Noise sources such as mechanical plant, <u>should</u> not be located near bedrooms or adjacent existing dwellings. Noise sensitive rooms and secluded private open spaces of new dwellings and residential buildings <u>should</u> take account of noise sources on immediately adjacent properties. Dwellings and residential buildings close to busy roads, railway lines or industry <u>should</u> be designed to limit noise levels in habitable rooms.	
<b>Clause 55.05-1</b>  <b>Accessibility Objective</b>  To encourage the consideration of the needs of people with limited mobility in the design of developments	<b>Standard B25</b>  The dwelling entries of the ground floor of dwellings and residential buildings <u>should</u> be accessible or able to be easily made accessible to people with limited mobility	The access to the building will be accessible. Lift access is provided from the basement. Complies
<b>Clause 55.05-2</b>  <b>Dwelling Entry Objective</b>  To provide each dwelling or residential building with its own sense of identity	<b>Standard B26</b>  Entries to dwellings and residential buildings <u>should</u> : <ul style="list-style-type: none"> <li>• Be visible and easily identifiable from streets and other public areas; and</li> <li>• Provide shelter, a sense of personal address and a transitional space around the entry.</li> </ul>	Complies
<b>Clause 55.05-3</b>  <b>Daylight to New Windows Objective</b>  To allow adequate daylight into new habitable room windows	<b>Standard B27</b>  A window in a habitable room <u>should</u> be located to face: <ul style="list-style-type: none"> <li>• an outdoor space clear to the sky or a light court with a minimum area of 3m<sup>2</sup> and minimum dimension of 1m, not including land on an abutting lot, or a verandah provided it is open for at least 1/3<sup>rd</sup> of its perimeter, or a carport provided it has two or more open sides and is open for at least 1/3<sup>rd</sup> of its perimeter.</li> </ul>	Complies
<b>Clause 55.05-4</b>  <b>Private Open Space Objective</b>  To provide for adequate private open space for the reasonable recreation and service needs of residents	<b>Summary of Standard B28</b>  A dwelling or residential building <u>should</u> have private open space of: <ul style="list-style-type: none"> <li>• 40m<sup>2</sup> with one part to be secluded private open space at the side or rear with a minimum area of 25m<sup>2</sup> and convenient access from a living room, or</li> <li>• A balcony of 8 square metres with a minimum width of 1.6 metres and convenient access from a living room, or</li> <li>• A roof-top area of 10 square metres with a minimum width of 2 metres and convenient access from a living room</li> </ul>	Each dwelling has good outdoor open space and balconies the smallest being 7.11m <sup>2</sup> . Complies with the objective



<p><b>Clause 55.05-5</b></p> <p><b>Solar Access to Open Space Objective</b></p> <p>To allow solar access into the secluded private open space of new dwellings and residential buildings</p>	<p><b>Standard B29</b></p> <p>The private open space <u>should</u> be located on the north side of the dwelling or residential building if appropriate.</p> <p>The southern boundary of secluded private open space <u>should</u> be setback from any wall on the north of the space at least <math>(2 + 0.9h)</math>, where 'h' is the height of the wall.</p>	<p>Complies</p>
<p><b>Clause 55.05-6</b></p> <p><b>Storage Objective</b></p> <p>To provide adequate storage facilities for each dwelling</p>	<p><b>Standard B30</b></p> <p>Each dwelling <u>should</u> have convenient access to at least 6m<sup>3</sup> of externally accessible, secure storage space.</p>	<p>Ample storage areas are provided</p> <p>Complies</p>
<p><b>Clause 54.06-1</b></p> <p><b>Design detail objective</b></p> <p>To encourage design detail that respects the existing or preferred neighbourhood character.</p>	<p><b>Standard B31</b></p> <p>The design of buildings <u>should</u> respect the existing or preferred neighbourhood character, including:</p> <ul style="list-style-type: none"> <li>• Facade articulation and detailing;</li> <li>• Window and door proportions;</li> <li>• Roof form; and</li> <li>• Verandahs, eaves and parapets.</li> </ul> <p>Garages and carports should be visually compatible with the development and the existing or preferred neighbourhood character.</p>	<p>The building has a varied façade of brick and cladding in a varied treatment with vertical feature elements in the façade.</p> <p>Complies</p>
<p><b>Clause 55.06-2</b></p> <p><b>Front Fences Objective</b></p> <p>To encourage front fence design that respects the existing or preferred neighbourhood character</p>	<p><b>Standard B32</b></p> <p>The design of front fences <u>should</u> complement the design of the dwelling or residential building and any front fences on adjoining properties.</p> <p>A front fence within 3m of a street <u>should</u> not exceed:</p> <ul style="list-style-type: none"> <li>• 2m height for streets in a Road Zone, Category 1; or</li> <li>• 1.5m height for any other street.</li> </ul>	<p>The front fence will be a 1.5m high masonry and palisade fence.</p> <p>Comp</p>
<p><b>Clause 55.06-3</b></p> <p><b>Common Property Objectives</b></p> <p>To ensure that communal open space, car parking, access areas and site facilities are practical, attractive and easily maintained;</p> <p>To avoid future management difficulties in areas of common ownership</p>	<p><b>Standard B33</b></p> <p>Developments <u>should</u> clearly delineate public, communal and private areas.</p> <p>Common property where provided, <u>should</u> be functional and capable of efficient management.</p>	<p>Complies</p>
<p><b>Clause 55.06-4</b></p> <p><b>Site Services Objectives</b></p> <p>To ensure that site services can be</p>	<p><b>Standard B34</b></p> <p>The design and layout of dwellings and residential buildings <u>should</u> provide sufficient space and facilities for services</p>	<p>Complies</p>

<p>installed and easily maintained; To ensure that site facilities are accessible, adequate and attractive</p>	<p>to be installed and maintained efficiently and economically.</p> <p>Bin and recycling enclosures, mailboxes and other site facilities should be adequate in size, durable, waterproof and blend in with the development.</p> <p>Bin and recycling enclosures should be located for convenient access by residents. Mailboxes should be provided and located for convenient access as required by Australia Post.</p>	
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## 5. CONCLUSION

The proposed development demonstrates its appropriateness for the site and compliance with the Glen Eira Planning Scheme as well as the absence of any adverse impacts on adjoining properties.

The development will implement the local planning policy directions toward providing a variety in housing type. Significantly the scale of the proposal is moderated in light of the surrounding context and it is an appropriate and attractive infill development.

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Traffic Engineers and Transport Planners

Our Reference: G18836L-01A

6 December 2016

AC Welland McKinnon Pty Ltd  
Suite 1212 St Kilda Towers  
1 Queens Road  
MELBOURNE VIC 3004

Attention: Mandy Man

ADVERTISED PLAN

7 OF 29  
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Dear Sir,

## 134-138 McKinnon Road, McKinnon – Proposed Residential Development Traffic Engineering Assessment

### Introduction

Further to your instructions, please find following our review of a proposed residential development 134-138 McKinnon Road, McKinnon.

The following report provides a traffic engineering assessment of traffic and parking issues associated with the development.

### Proposal

It is proposed to develop the subject site as a three-storey residential development comprising 21 dwellings as follows:

- One-bedroom dwellings: 1 no.
- Two-bedroom dwellings: 18 no.
- Three-bedroom dwellings: 2 no.
- Total: 21 no.

Twenty six car parking spaces are to be provided on site within a basement car park, comprising 24 independent car stacker spaces and two standard spaces. All car stacker parking spaces are to be allocated to residents of the proposed development and the two standard spaces are to be allocated to visitors.

Vehicle access to the site is to be taken via the rear right of way. The existing crossovers to McKinnon Road will be reinstated with kerb and channel. As a result, three additional on-street parking spaces will be created along the site's McKinnon Road frontage.



Twenty one bicycle spaces for use by residents are to be provided on site within the basement car park, via wall mounted bicycle racks. An additional two bicycle parking spaces are to be provided for visitors within the entry porch at ground level.

A "passing area" has been created at the rear of the site to facilitate two way traffic movements along the rear right of way and refuse collection.

Plans of the proposed development are attached at Appendix A.

## Existing Conditions

### Subject Site

The subject site is located on the south side of McKinnon Road, McKinnon, approximately 90m to the west of Nicholson Street as shown in the locality plan at Figure 1. It is situated at the edge of the McKinnon Village Shopping Centre.

The subject site is rectangular in shape and comprises three separate lots; namely, 134, 136 and 138 McKinnon Road. Each lot currently accommodates a single storey residential dwelling, with vehicle access to each lot taken via separate crossovers to McKinnon Road.



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Figure 1: Locality Map

### Land Use

The site is situated within a General Residential Zone - Schedule 1 (GRZ1) under the Glen Eira Planning Scheme as shown in Figure 2. The land is also within a Parking Overlay - Schedule 2 (PO2). Despite being within the Parking Overlay, the land use does not trigger the operation of the overlay as the overlay relates specifically to Student Housing.



Existing land uses surrounding the subject site comprise a mixture of residential and commercial/retail uses. The property immediately to the west is a dental practice. The property immediately to the east is a mixed use development (commercial and residential uses). McKinnon Railway Station is located approximately 160m east of the subject site.



Source: land.vic.gov.au

Figure 2: Zoning Map

### Road Network

McKinnon Road is a Major Council Road which is orientated in an east-west direction. In the vicinity of the subject site, McKinnon Road has an undivided carriageway which accommodates a single lane of through traffic and indented formal kerbside parallel parking in each direction of travel.

On-street parking along McKinnon Road is generally short term in nature

McKinnon Road is shown in Photographs 1 and 2.



Photograph 1: McKinnon Road - View East Adjacent to Subject Site



Photograph 2: McKinnon Road - View West Adjacent to Subject Site

A rear **right of way (RoW)** runs along the site's southern boundary and extends between Nicholson Street to the east and Wheatley Road to the west. The RoW is 3.2m wide and provides vehicle access to a number of properties which front McKinnon Road (to the north of the RoW) and Lees Street (to the south of the RoW).

The RoW is shown in Photographs 3 and 4.



Photograph 3: Right of Way - View East Adjacent to Subject Site



Photograph 4: Right of Way - View West Adjacent to Subject Site

#### Traffic Conditions

Traffix Group undertook traffic counts at the intersection of the RoW and Nicholson Street. The counts were undertaken on Friday 19 June, 2015 from 7:30am to 9:30am and from 4:30pm to 6:30pm.

A summary of the peak hour traffic counts is shown in Figure 3. The AM peak hour period occurred from 8:30am to 9:30am and the PM peak hour period occurred from 5:30pm to 6:30pm.



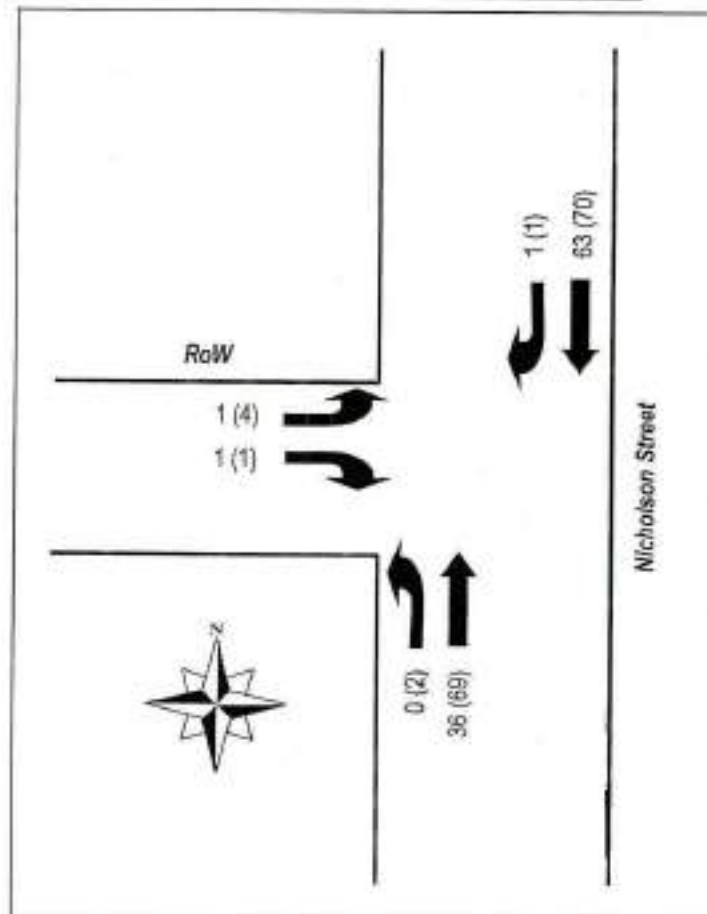


Figure 3: AM (PM) Peak Hour Traffic Counts Summary

## Car Parking Assessment

### Statutory Car Parking Assessment

Clause 52.06-5 of the Planning Scheme sets out the number of car parking spaces required for a use. The statutory car parking requirement of the proposed development is set out in Table 1.

Table 1: Statutory Car Parking Requirements

	Use	No.	Rate	Requirement
Dwelling	One or two-bedroom dwellings	19	1 car space to each dwelling	19 spaces
	Three-bedroom dwellings	2	2 car spaces to each dwellings	4 spaces
	Visitors	21	1 car space to every 5 dwellings	4 spaces
Total				27 spaces

The proposed development has a statutory requirement to provide 27 car parking spaces.

The development plan shows 26 car parking spaces. Twenty four of these spaces are to be provided within the stacker system and are to be allocated to residents. The two standard spaces are to be provided for visitors. The application satisfies the car parking requirement for residents, but seeks a reduction of the visitor car parking requirement.

#### **Reducing the Car Parking Requirement**

Clause 52.06-6 of the Planning Scheme allows for the statutory car parking requirement to be reduced.

For applications to reduce the car parking requirement, Clause 52.06-6 requires that a Car Parking Demand Assessment be undertaken to assess the car parking demand likely to be generated by the proposed use. Before granting a permit to reduce the number of spaces, the responsible authority must also consider a separate set of decision guidelines to determine whether it is appropriate to allow the provision of fewer spaces.

Planning Practice Note 22 - Using the Car Parking Provisions (June 2015) notes the following with regard to reducing the car parking requirement:

*"Clause 52.06-6 draws a distinction between the assessment of likely demand for parking spaces, and whether it is appropriate to allow the supply of fewer spaces. These are two separate considerations, one technical while the other is more strategic. Different factors are taken into account in each consideration."*

We have set out an assessment of the relevant decision guidelines below.

#### **Car Parking Demand Assessment**

##### Public Transport

The subject site is well served by public transport, with the following services operating nearby:

- **Bus Route 626** operates along McKinnon Road directly past the subject site. It provides a service between Middle Brighton Railway Station and Chadstone Shopping Centre via McKinnon and Carnegie.
- **McKinnon Railway Station** is located approximately 160m walking distance to the northeast of the subject site. It is served by train services operating on the Frankston railway line.

The available public transport services surrounding the subject site are shown in Figure 4.



Source: ptv.vic.gov.au

Figure 4: Public Transport Services

This level of public transport accessibility will provide visitors to the proposed development with an alternative to the private car.

#### Anticipated Parking Demands

##### Residential Visitors

For the purposes of this assessment, the statutory requirement of four car spaces is considered to be representative of the likely car parking demands associated with residential visitors. Visitor car parking demands tend to peak during evenings and on weekends. During business hours, visitor car parking demands are typically around half the evening car parking demand (i.e. in this instance, two spaces). Visitor car parking demands are typically short term in nature.

Two visitor spaces are provided within the basement carpark which will accommodate the daytime visitor parking demand. The development is expected to be reliant on up to two off-site parking spaces on weekends and during the evening.

##### Allowing Fewer Car Spaces

Traffix Group undertook car parking surveys within approximately 200m walking distance from the subject site. The survey area is shown in Figure 5.





Figure 5: Parking Survey Area

The parking occupancy surveys were undertaken at the following times:

- Friday 12 June, 2015 at 2:00pm;
- Friday 19 June, 2015 at 7:00am, 9:30am, 4:00pm and 6:30pm; and
- Saturday 20 June, 2015 at 12:00noon and 7:00pm.

Details of the surveys, including parking restrictions and occupancies, are attached at Appendix B.

In summary, the parking surveys found the following:

Friday 19 June, 2015 at 7:00am:

- There were 186 suitable<sup>1</sup> car spaces available within the survey area.
- At 7:00am, 89 spaces were occupied (48% occupancy), leaving 97 vacant spaces within the survey area.
- Of the 97 vacant spaces, 35 spaces were available along McKinnon Road.

Friday 19 June, 2015 at 9:30am:

- There were 182 suitable car spaces available within the survey area.
- At 9:30am, 121 spaces were occupied (66% occupancy), leaving 61 vacant spaces within the survey area.
- Of the 61 vacant spaces, 20 spaces were available along McKinnon Road.

<sup>1</sup> Suitable car parking spaces are spaces that can be utilised by visitors at the specified time periods and exclude Loading Zone and Bus Zone parking restrictions. The number of suitable car parking spaces varies throughout the day due to varying car parking restrictions.

Friday 12 June, 2015 at 2:00pm:

- There were 182 suitable car spaces available within the survey area.
- At 2:00pm, 130 spaces were occupied (71% occupancy), leaving 52 vacant spaces within the survey area.
- Of the 52 vacant spaces, 18 spaces were available along McKinnon Road.

Friday 19 June, 2015 at 4:00pm:

- There were 182 suitable car spaces available within the survey area.
- At 4:00pm, 123 spaces were occupied (68% occupancy), leaving 59 vacant spaces within the survey area.
- Of the 59 vacant spaces, 16 spaces were available along McKinnon Road.

Friday 19 June, 2015 at 6:30pm:

- There were 194 suitable car spaces available within the survey area.
- At 6:30pm, 124 spaces were occupied (64% occupancy), leaving 70 vacant spaces within the survey area.
- Of the 70 vacant spaces, 20 spaces were available along McKinnon Road.

Saturday 20 June, 2015 at 12:00noon:

- There were 186 suitable car spaces available within the survey area.
- At 12:00noon, 114 spaces were occupied (61% occupancy), leaving 72 vacant spaces within the survey area.
- Of the 72 vacant spaces, 17 spaces were available along McKinnon Road.

Saturday 20 June, 2015 at 7:00pm:

- There were 194 suitable car spaces available within the survey area.
- At 7:00pm, 132 spaces were occupied (68% occupancy), leaving 62 vacant spaces within the survey area.
- Of the 62 vacant spaces, ten spaces were available along McKinnon Road.

The survey results indicate that there is ample parking available within the area to cater for the likely parking demands associated with visitors of the proposed development.

Three new on-street car spaces will be created along the site's McKinnon Road frontage, as a result of reinstating the three existing crossovers with kerb and channel. These three additional on-street car parking spaces will accommodate the anticipated evening and weekend visitor car parking demand of two off-site spaces. In effect, the proposed development will result in an increase in the car parking supply even after allowing for off-site demands.

Based on the foregoing, we are satisfied that there is sufficient scope for the likely visitor car parking demands to be accommodated on street within the nearby car parking resources, without any significant detrimental impacts on the overall availability of car parking within the area.

A reduction in the statutory car parking requirements is therefore justified.



## Car Parking Layout

The proposed car park layout and access arrangements have been assessed against the relevant standards within the Glen Eira Planning Scheme and, where applicable, the Australian Standard for off-street parking facilities (AS/NZS 2890.1:2004). The assessment reveals the following:

### Accessways

- The proposed accessway is 3.6m wide between walls, which meets both the Planning Scheme and AS/NZS 2890.1:2004 requirements.
- All vehicles can enter and exit the site in a forwards direction.
- Given that the site does not connect to a road in a road zone, no passing bay is provided at the entrance to the site.
- A minimum headroom clearance of 2.3m is to be achieved along the accessway. This exceeds the requirements of AS/NZS 2890.1:2004 and the Planning Scheme.
- A sight triangle is provided on both sides of the proposed access point in accordance with the Planning Scheme.

### Car Spaces

- All car spaces have been provided in accordance with the requirements of the Planning Scheme. Specifically, the parking spaces are dimensioned as 2.6m wide by 5.4m long accessed via a 6.4m wide aisle.
- A 1m blind aisle extension is provided for the car spaces along the southern boundary.
- A minimum headroom clearance of 2.2m is to be achieved above all car spaces and circulation aisles. This satisfies the requirements of AS/NZS 2890.1:2004 and exceeds the requirements of the Planning Scheme.

### Gradients

- Ramp grades for the first 5m from the property boundary do not exceed 1:10.
- A maximum ramp grade of 1:4 is provided, which is accords with the Planning Scheme requirement for private car parks.
- Appropriate ramp transitions have been designed at the top and bottom of the ramp to prevent scraping.

### Car Stackers

- Twenty four independent car stacker spaces are to be provided within the basement car park.
- All car stacker spaces are to be provided via Multibase 2072 (or similar) systems. The relevant specification sheets for these systems are attached at Appendix C.
  - The Multibase 2072 system accommodates two levels of parking (upper and lower level) and allows for all car spaces to be accessed independently via a pit.
  - The proposed car stacker spaces are 5.4m long, which is sufficient to accommodate vehicles up to 5.2m in length (i.e. the B99 design vehicle).



- o A usable platform width of at least 2.4m is provided for all spaces which accords with AS/NZS 2890.1:2004.
- o A minimum headroom clearance of 3.8m is to be provided above the car stacker spaces. This headroom clearance is sufficient to accommodate vehicles up to 1.8m in height on both parking levels.

We note that the Planning Scheme specifies that at least 25% of all car stacker spaces should accommodate a vehicle clearance height of at least 1.8m. The proposed car stacker spaces satisfy this requirement.

#### **Manoeuvrability**

Swept path diagrams have been prepared, attached at Appendix D, showing suitable access by the B85 design vehicle (as specified in AS/NZS 2890.1:2004) to the critical parking spaces.

#### **Conclusion**

Based on the foregoing assessment, we are satisfied that the proposed car parking layout and access arrangements are appropriate for the proposed development and will provide for convenient and accessible parking.

#### **Bicycle Facilities**

The statutory bicycle parking requirements for the proposed development are set out under Clause 52.34 of the Planning Scheme.

The requirement for bicycle parking for dwellings is triggered when the building is four or more storeys in height. The proposed development is to be three storeys in height. Accordingly, there is no requirement for bicycle parking.

Notwithstanding this, 21 bicycle spaces are to be provided within the basement car park, via wall mounted rails, for use by residents. The proposed bicycle spaces are to be provided via 'Cycloc Endo' bicycle parking systems.

The bicycle spaces are to be spaced at a width of 320mm, in accordance with the specification sheets.

The Bicycle Victoria – Bicycle Parking Handbook requires an overall width of 2.7m from the wall for wall mounted bicycle racks. The provision of bicycle parking is provided within a width of 3.75m which exceeds the Bicycle Victoria requirement.

In addition, four bicycle parking spaces are to be provided for visitors within the entry porch at ground level via a Cora Expo 4506 (or similar) bicycle rack. No dimensions are shown on the plans. These spaces should be provided in accordance with the Cora specification.

On this basis, we consider that the proposed bicycle parking provision is appropriate.

## **Traffic Considerations**

### **Traffic Generation and Distribution**

Given the site's location and the size of the proposed apartments, we consider that a traffic generation rate of four vehicles per day (vpd) per dwelling is appropriate in this instance. Peak hour traffic generation will be in the order of 10% of the daily traffic generation, i.e. 0.4 vehicles per hour (vph) per dwelling.

For the proposed development, this equates to 84 vehicle trip ends (vte) per day, with eight vte occurring in each of the commuter peak hours.

This level of traffic is anticipated to be generated to/from Nicholson Street, via the RoW.

### **Traffic Impact**

#### RoW

As shown in Figure 3, the existing traffic volumes along the RoW are low, with a maximum two-way volume of eight vehicles recorded at the Nicholson Street end during peak periods.

The proposed development is anticipated to generate an additional eight movements during peak hours, equal to on average one vehicle in either direction every 7.5 minutes. This level of traffic is negligible and will not have any detrimental impacts on the operation of the RoW.

The maximum two-way volume in the RoW at Nicholson Street post-development will therefore be 16 vehicles during peak hours. The RoW is 3.2m wide and operates as a single lane, two-way road.

Clause 3.3.2 in AS/NZS 2890.1:2004 provides commentary and guidance on the use of access driveways/laneways. The clause states that, as a guide, 30 or more movements in a peak hour (in and out combined) would usually require provision for two vehicles to pass on the driveway. The anticipated peak hour traffic volume is lower than the capacity suggested by the Australian Standard.

Nonetheless, a passing area is provided in the vicinity of the site access to enable vehicles travelling in opposite directions along the RoW to pass each other.

Accordingly, we are satisfied that the RoW will cater for the anticipated traffic volumes likely to be generated by the proposed development and that the proposed passing area will facilitate traffic movements along the RoW.

#### Nicholson Street/RoW Intersection

Based on the traffic counts conducted at the intersection of Nicholson Street and the RoW (as shown in Figure 3), in the order of 150 vehicles currently travel through this intersection during peak hours.

An additional eight vehicles are anticipated to travel through this intersection during peak hours as a result of the proposed development. This is equivalent to, on average, one additional vehicle every 7.5 minutes. This volume of traffic is negligible and will not have any detrimental impacts on the capacity and operation of the intersection.



### **Conclusion**

Based on the foregoing assessment, we are satisfied that the anticipated level of traffic that is likely to be generated by the proposed development will not have any significant detrimental impacts on the capacity and operation of the RoW, Nicholson Street and the surrounding road network and intersections.

### **Waste Collection**

Waste collection is proposed from within the basement car park via a private contractor using a 6.4m long refuse collection vehicle.

Swept path diagrams of the Waste Wise Mini Rear Loader vehicle accessing the basement car park have been prepared and are attached at Appendix D. The vehicle is shown to be able to enter and exit the site in a forwards direction.

We are therefore satisfied that the waste collection arrangements are appropriate for the proposed development.

### **Conclusions**

Having undertaken a detailed traffic engineering assessment of the proposed residential development at 134-138 McKinnon Road, McKinnon, we are of the opinion that:

- a) The proposed car parking provision satisfies the statutory car parking requirement for residents of the proposed development.
- b) There is sufficient scope for the likely parking demands associated with residential visitors to be accommodated on street within the nearby parking resources, without any detrimental impacts on the overall availability of parking within the area.
- c) The proposed car parking layout and access arrangements are appropriate for the proposed development and will provide for convenient and accessible parking.
- d) The proposed bicycle parking provision is appropriate.
- e) The anticipated level of traffic that is likely to be generated by the proposed development will not have any significant detrimental impacts on the capacity and operation of the RoW, Nicholson Street and the surrounding road network and intersections.
- f) There are no traffic engineering reasons why a planning permit should not be issued for the proposed development.

Yours faithfully,

A handwritten signature in blue ink, appearing to read 'Don Robertson'.

Don Robertson

Senior Consultant

**TRAFFIX GROUP PTY LTD**

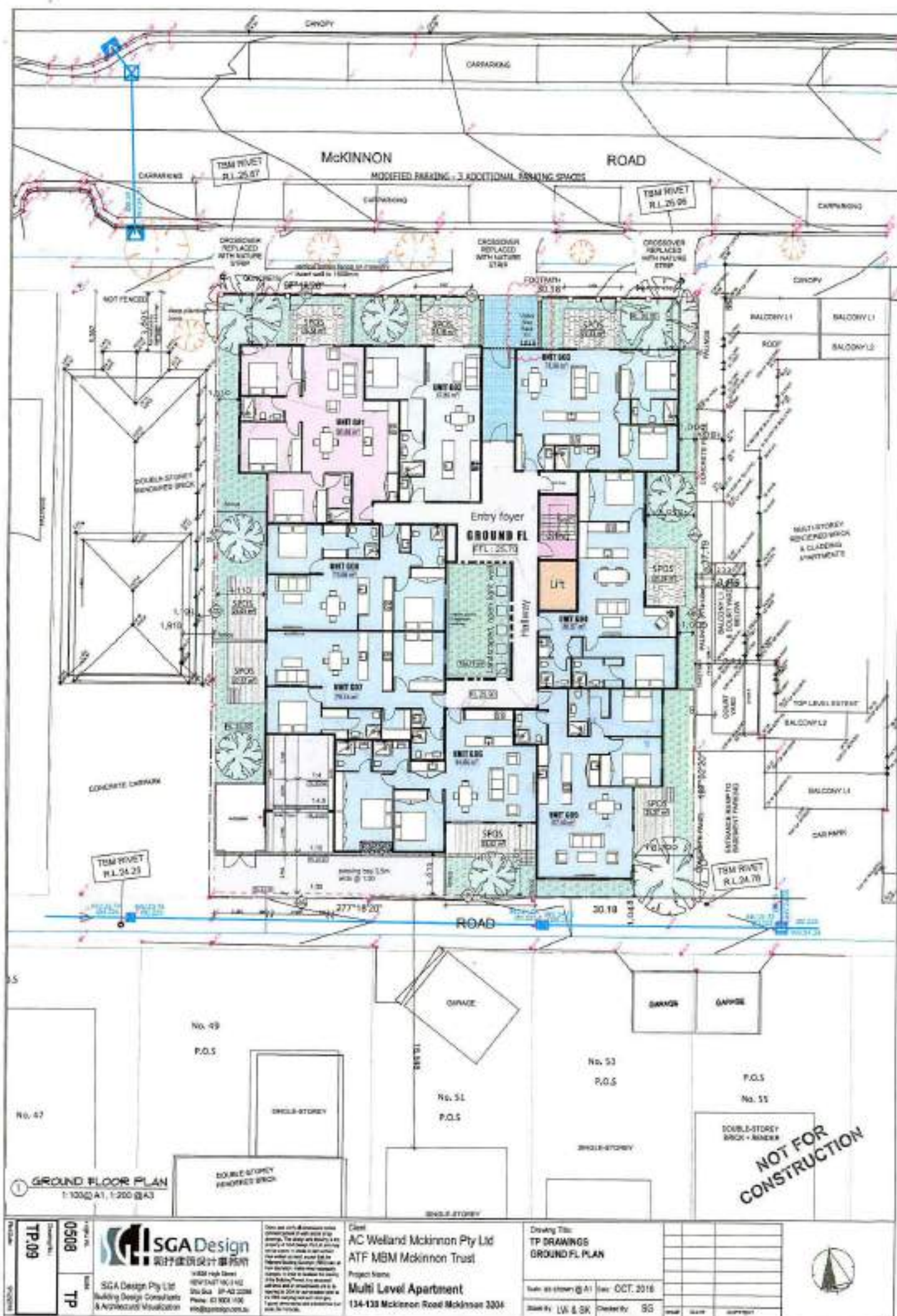




## Appendix A: Development Plans







SGA Design Pty Ltd  
Building Design Consultants  
& Architectural Visualisation

1000 High Street  
Melbourne VIC 3004  
Ph: 03 9594 2288  
Fax: 03 9594 1106  
info@sgadesign.com.au

Client: AC Welland McKinnon Pty Ltd  
ATF MSM McKinnon Trust  
Project Name: Multi Level Apartment  
134-138 McKinnon Road McKinnon 3004

Drawing Title:  
TP DRAWINGS  
GROUND FL PLAN

Scale: as shown @ A1  
Date: OCT. 2016  
Drawn By: LW & SK  
Checked By: SG







## Appendix B: Parking Surveys

134-138 McKinnon Road, McKinnon  
Parking Occupancy Survey

GRP18838

Survey Date: See below



LOCATION	RESTRICTION	Capacity	NO. OF CARS PARKED						
			Friday 12/8/15 2:00pm	Friday 18/8/15 7:00am	Friday 18/8/15 9:30am	Friday 18/8/15 4:00pm	Friday 19/8/15 8:30am	Saturday 20/8/15 12:00pm	Sunday 22/8/15 12:00pm
McKinnon Road (North Side)									
Wheatley Road to Wattle Grove	No Stopping	0	0	8	0	0	0	8	0
	1HP 8am-6pm Mon-Fri	2	1	8	0	1	0	4	8
	2P 8am-6pm Mon-Sat	10	8	8	8	8	7	7	10
Wattle Grove to Elm Grove	No Stopping	0	0	0	0	0	0	0	0
	No Stopping	0	0	0	0	0	0	0	0
	1P 8am-6pm Mon-Fri, 8am-12noon Sat	8	4	2	7	3	0	0	5
Elm Grove to Glen Oshea Avenue	No Stopping	0	0	0	0	0	0	0	0
	No Stopping	0	0	0	0	0	0	0	0
	1P 8am-12noon, 12noon-6pm	4	5	3	8	0	8	5	6
	1HP 8am-12noon, 12noon-6pm	2	2	1	1	2	1	2	2
Glen Oshea Avenue to Railway Lines	No Stopping	0	0	0	0	0	0	0	0
	No Stopping	0	0	0	0	0	0	0	0
	1P 8am-6pm	4	4	1	4	4	4	3	1
	No Stopping	0	0	0	0	0	0	0	0
Railway Lines to Station Avenue	No Stopping	0	0	0	0	0	0	0	0
	No Stopping	0	0	0	0	0	0	0	0
	Bus Zone	-	-	-	-	-	-	-	-
	1HP 8am-6pm	1	1	1	0	0	0	0	0
	2P 8am-6pm Mon-Fri, 8am-12noon Sat	2	2	0	2	2	1	2	1
McKinnon Road (South Side)									
Bent Street to Railway Lines	No Stopping	0	0	0	0	0	0	0	0
	1P 8am-6pm Mon-Fri, 8am-12noon Sat	1	1	0	0	1	0	1	1
	Bus Zone	-	-	-	-	-	-	-	-
Railway Lines to Nicholson Street	No Stopping	0	0	0	0	0	0	0	0
Nicholson Street to Pedestrian Operated Signal	No Stopping	0	0	0	0	0	0	0	0
	No Stopping	0	0	0	0	0	0	0	0
	1HP 8am-6pm	8	3	4	0	4	2	4	3
Pedestrian Operated Signal to 133 No. 138 McKinnon Road	No Stopping	0	0	0	0	0	0	0	0
	No Stopping	0	0	0	0	0	0	0	0
	2P Disabled	1	0	0	1	1	0	0	1
1P 8am-6pm Mon-Fri, 8am-12noon Sat		2	1	0	1	2	2	2	2



**134-138 McKinnon Road, McKinnon**  
**Parking Occupancy Survey**

GRP18838

Survey Date: See below



LOCATION	RESTRICTION	Capacity	NO. OF CARS PARKED						
			Friday 12/5/15 2:00am	Friday 19/5/15 7:00am	Friday 19/5/15 6:30am	Friday 19/5/15 4:30pm	Friday 19/5/15 6:30pm	Saturday 20/5/15 12:00pm	Saturday 20/5/15 7:00pm
EB No. 138 McKinnon Road WB No. 134 McKinnon Road	1P 8am-6pm Mon-Fri, 8am-12noon Sat	3	2	0	2	2	3	3	3
WB No. 134 McKinnon Road to WB No. 116 McKinnon Road	1P 8am-6pm Mon-Fri, 8am-12noon Sat	7	3	3	3	2	7	4	7
	Bus Zone 8am-6pm Mon-Sat	4	0	5	5	5	5	5	4
WB No. 116 McKinnon Road to WB No. 102 McKinnon Road	Unrestricted	5	4	4	5	5	5	4	5
	Unrestricted	7	7	7	7	5	4	5	5
WB No. 102 McKinnon Road to Woodside Road	No Stopping	0	0	0	0	0	0	0	0
McKinnon Road	Capacity	-	66	66	60	60	70	66	75
	No. Cars Parked	-	49	31	47	62	40	41	60
	No. Vehicle Spaces	-	14	24	20	14	20	17	10
	Percentage Occupancy	-	73%	47%	77%	100%	57%	61%	80%
<b>Glen Osage Avenue (West Side)</b>									
McKinnon Road to Lanesway	No Stopping	0	0	0	0	0	0	0	0
	1/2P 8am-5pm Mon-Fri, 8am-12noon Sat	1	1	0	1	1	1	0	1
	2P 8am-6pm Mon-Fri, 8am-12noon Sat	4	4	3	3	4	4	4	4
	No Stopping	0	0	0	0	0	0	0	0
Lanesway to NB No. 7 Glen Osage Avenue	No Stopping	0	0	0	0	0	0	0	0
	2P 8am-6pm Mon-Fri, 8am-12noon Sat	5	6	6	6	5	5	4	6
NB No. 7 Glen Osage Avenue to NB No. 9a Glen Osage Avenue	2P 8am-6pm Mon-Fri, 8am-12noon Sat	2	2	1	1	1	0	2	0
<b>Glen Osage Avenue (East Side)</b>									
NB No. 8 Glen Osage Avenue to SB No. 9 Glen Osage Avenue	2P 8am-6pm Mon-Fri, 8am-12noon Sat	3	2	2	1	1	3	2	2
SB No. 9 Glen Osage Avenue to McKinnon Road	2P 8am-6pm Mon-Fri, 8am-12noon Sat	2	2	1	2	1	0	2	2
	No Stopping	0	0	0	0	0	0	1	1
	2P 8am-6pm Mon-Fri, 8am-12noon Sat	2	2	1	3	3	3	2	2
	No Stopping	0	0	0	0	0	0	0	0
Glen Osage Avenue	Capacity	-	23	23	23	20	24	20	23
	No. Cars Parked	-	19	12	17	16	16	11	18
	No. Vehicle Spaces	-	1	8	2	4	6	2	2
	Percentage Occupancy	-	83%	52%	74%	80%	67%	55%	78%

**134-138 McKinnon Road, McKinnon**  
**Parking Occupancy Survey**  
 GRP18838

Survey Date: See below



LOCATION	RESTRICTION	Capacity	NO. OF CARS PARKED						
			Friday 12/5/18 12:00pm	Friday 12/5/18 2:00pm	Friday 12/5/18 10:00am	Friday 12/5/18 4:00pm	Friday 12/5/18 10:00am	Saturday 24/2/18 12:00noon	Saturday 24/2/18 7:00pm
McKinnon Street (East Side)									
McKinnon Road to Opposite Lees Street	No Stopping	0	0	0	0	0	0	0	0
	2P 8am-6pm Mon-Fri, 8am-12noon Sat	4	3	2	3	2	2	2	4
	4P 8am-6pm Mon-Fri	6	4	1	5	2	3	4	1
Opposite Lees Street to Opposite Division Street	Unrestricted	18	18	0	15	16	0	9	9
McKinnon Street (West Side)									
McKinnon Street to Lees Street	No Stopping	0	0	0	0	0	0	0	0
	2P 8am-6pm Mon-Fri, 8am-12noon Sat	7	0	0	1	1	2	0	2
	No Stopping	0	0	0	0	0	0	0	0
Lees Street to McKinnon Road	No Stopping	0	0	0	0	0	0	0	0
	Work Zone 7am-5pm Mon-Sat	4	4	4	4	1	2	1	3
	2P 8am-6pm Mon-Fri, 8am-12noon Sat	2	1	1	1	1	1	2	0
	Loading Zone	0*	0*	0*	0*	0*	0*	0*	0*
	No Stopping	0	0	0	0	0	0	0	0
	Capacity	-	37	37	37	37	41	41	41
McKinnon Street	No. Cars Parked	-	34	18	23	22	17	14	19
	No. Vacant Spaces	-	3	19	14	15	24	27	22
	Percentage Occupancy	-	92%	49%	62%	59%	41%	34%	46%
Lees Street (South Side)									
McKinnon Street to ED No. 48 Lees Street	No Stopping	0	0	0	0	0	0	0	0
	Unrestricted	5	3	4	4	4	3	4	4
ED No. 48 Lees Street to WB No. 48 Lees Street	Unrestricted	3	2	4	2	4	4	2	4
Lees Street (North Side)									
WB No. 31 Lees Street to McKinnon Street	2P 8am-6pm Mon-Fri, 8am-12noon Sat	10	8	9	8	7	1	5	3
	No Stopping	0	0	0	0	0	0	0	0
Lees Street	Capacity	-	18	18	18	17	18	18	18
	No. Cars Parked	-	11	11	11	11	14	11	11
	No. Vacant Spaces	-	7	7	7	6	4	7	7
	Percentage Occupancy	-	61%	61%	61%	65%	78%	61%	61%



**134-138 McKinnon Road, McKinnon**  
**Parking Occupancy Survey**  
 GRP18836

Survey Date: See below



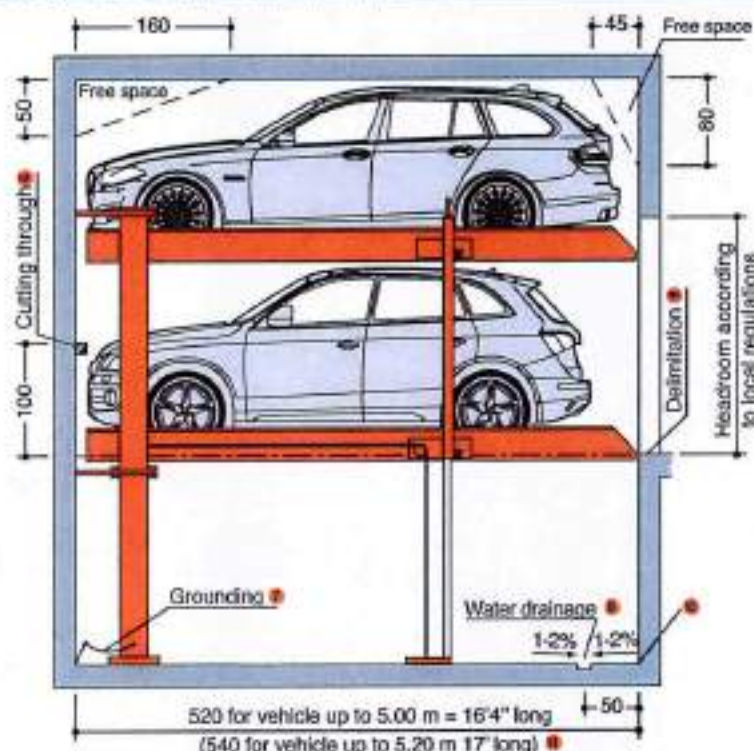
LOCATION	RESTRICTION	Capacity	NO. OF CARS PARKED						
			Friday 12/5/15 2:00pm	Friday 19/5/15 7:00am	Friday 19/5/15 9:30am	Friday 19/5/15 4:00pm	Friday 19/5/15 6:30pm	Saturday 20/5/15 12:00noon	Saturday 20/5/15 5:00pm
Wattle Grove (West Side)									
McKinnon Road to NB No. 1 Wattle Grove	No Stopping	6	0	0	6	0	6	0	6
	Unrestricted	7	6	5	7	6	6	4	2
Wattle Grove (East Side)									
NB No.2 Wattle Grove to McKinnon Road	2P Sun-4pm Mon-Fri, 8am-12noon Sat	6	3	3	3	2	4	3	3
	No Stopping	6	0	0	6	0	0	0	6
Wattle Grove	Capacity	-	12	18	12	12	11	12	12
	No. Cars Parked	-	8	7	10	8	11	4	4
	No. Vacant Spaces	-	4	11	2	4	2	8	8
	Percentage Occupancy	-	67%	39%	83%	67%	82%	33%	33%
Elm Grove (West Side)									
McKinnon Road to SB No. 1 Elm Grove	No Stopping	0	0	0	0	0	0	0	0
	No Parking Sun-4pm Mon-Fri, 8am-12noon Sat	4	6	6	0	0	2	1	3
	No Stopping	0	6	6	0	0	0	0	0
SB No. 1 Elm Grove to NB No. 3 Elm Grove	No Stopping	0	6	6	0	0	0	6	0
	2P Sun-4pm Mon-Fri, 8am-12noon Sat	5	5	1	1	2	4	2	4
NB No. 3 Elm Grove to NB No. 9 Elm Grove	Unrestricted	7	6	5	4	3	4	4	3
NB No. 9 Elm Grove to NB No. 11 Elm Grove	Unrestricted	2	1	0	0	0	0	0	0
Elm Grove (East Side)									
SB No. 14 Elm Grove to SB No. 8 Elm Grove	2P Sun-4pm Mon-Fri, 8am-12noon Sat	7	1	2	1	2	1	4	4
SB No. 8 Elm Grove to SB No. 4 Elm Grove	2P Sun-4pm Mon-Fri, 8am-12noon Sat	4	4	0	2	2	2	2	4
SB No. 4 Elm Grove to McKinnon Road	2P Sun-4pm Mon-Fri, 8am-12noon Sat	1	2	1	1	1	1	2	1
	No Stopping	8	0	0	0	0	0	1	0
	2P Sun-4pm Mon-Fri, 8am-12noon Sat	3	1	1	1	2	3	0	1
Elm Grove	Capacity	-	29	11	14	29	31	31	31
	No. Cars Parked	-	11	16	10	16	17	17	20
	No. Vacant Spaces	-	9	23	19	13	14	14	11
	Percentage Occupancy	-	38%	145%	71%	55%	55%	55%	65%
Total On-Street Parking	Capacity	-	181	195	182	182	194	196	194
	No. Cars Parked	-	130	86	121	123	134	114	133
	No. Vacant Spaces	-	51	87	61	59	60	82	61
	Percentage Occupancy	-	72%	44%	66%	68%	69%	58%	69%

SB: South boundary  
 NB: North boundary  
 ME: East boundary  
 WB: West boundary



## Appendix C: Stacker Specifications

Garage without door (basement garage)



Dimensions

All space requirements are minimum finished dimensions.

Tolerances for space requirements  $\pm \frac{1}{2}$ .  
Dimensions in cm.

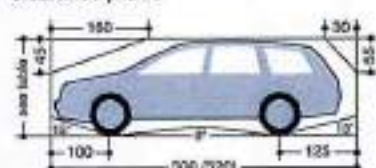
EB (single platform) = 2 vehicles  
DB (double platform) = 4 vehicles

Suitable for

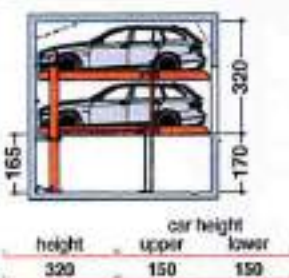
Standard passenger cars:  
Limousine, station wagon, SUV, van  
according to clearance and maximal  
surface load.

	Standard	Special <sup>•</sup>
width	190 cm <sup>•</sup>	190 cm <sup>•</sup>
weight	max. 2000 kg	max. 2600 kg
wheel load	max. 500 kg	max. 650 kg

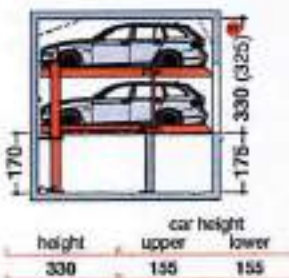
Clearance profile



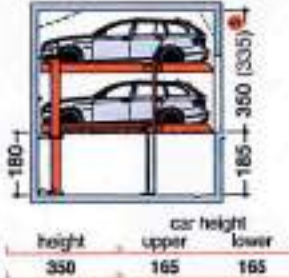
2072-163



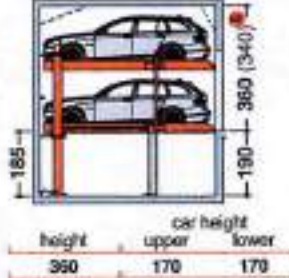
2072-170



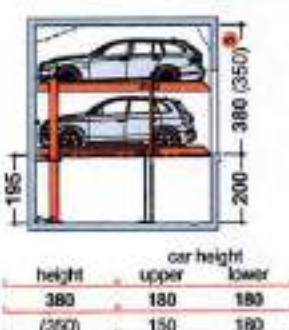
2072-180<sup>•</sup>



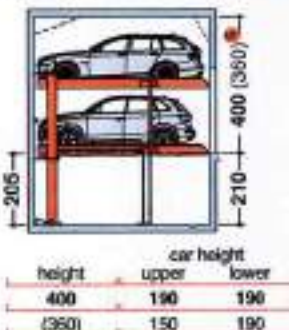
2072-189



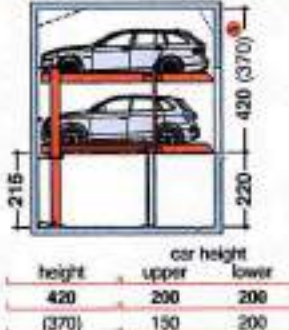
2072-195



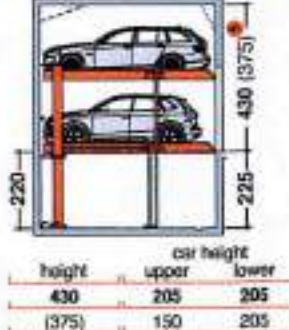
2072-205



2072-215



2072-220



- Standard type
- Special system: maximum load for extra charge.
- To follow the minimum finished dimensions, make sure to consider the tolerances according to VOB, part C (DIN 18330 and 18331) and the DIN 18202.
- Car width for platform width 230 cm. If wider platforms are used it is also possible to park wider cars.
- If a higher ceiling height is available higher cars can be parked.
- For dividing walls: cutting through 10 x 10 cm.
- Potential equalization from foundation grounding connection to system (provided by the customer).

- In compliance with DIN EN 14010, 10 cm wide yellow-black markings compliant to ISO 3864 must be applied by the customer to the edge of the pit in the entry area to mark the danger zone (see „load plan“ page 4).
- Slope with drainage channel and sump.
- At the transition section between pit floor and walls no hollow mouldings/coves are possible. If hollow mouldings/coves are required, the systems must be designed smaller or the pits accordingly wider.
- For convenient use of your parking space and due to the fact that the cars keep becoming longer we recommend a pit length of 540 cm.



Seite 1  
Section  
Dimensions  
Car data

Seite 2  
Width dim.  
without door

Seite 3  
Width dim.  
with door  
Function  
Approach

Seite 4  
Load plan  
Installation

Seite 5  
Electrical  
installation  
Technical  
data

Seite 6  
To be performed  
by the customer  
Description

## Width dimensions for garage without door (basement garage)

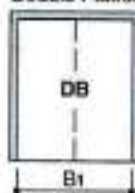
### Dividing walls

#### Single Platform (EB)



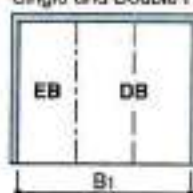
usable platform width	B1
230	260
240	270
250	280
260	290
270	300

#### Double Platform (DB)



usable platform width	B1
460	490
470	500
480	510
490	520
500	530
510	540
520	550
530	560
540	570

#### Single and Double Platform (EB + DB) – Example

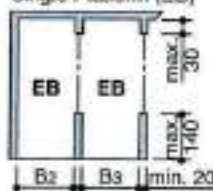


usable platform width	B1
230 + 460	750
240 + 470	770
250 + 480	790
260 + 500	810
270 + 500	830
270 + 510	840
270 + 520	850
270 + 530	860
270 + 540	870

Carriageway in  
accordance with  
local regulations

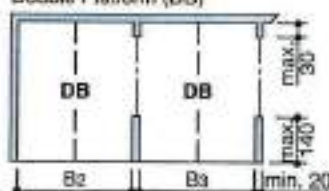
### Columns in pit

#### Single Platform (EB)



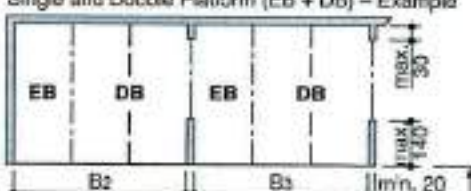
usable platform width	B2	B3
230	255	245
240	265	255
250	275	265
260	285	275
270	295	285

#### Double Platform (DB)



usable platform width	B2	B3
460	485	475
470	495	485
480	505	495
490	515	505
500	525	515
510	535	525
520	545	535
530	555	545
540	565	555

#### Single and Double Platform (EB + DB) – Example

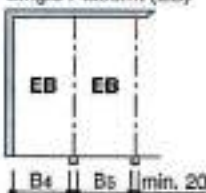


usable platform width	B2	B3
230 + 460	745	735
240 + 470	765	755
250 + 480	785	775
260 + 500	805	795
270 + 500	825	815
270 + 510	835	825
270 + 520	845	835
270 + 530	855	845
270 + 540	865	855

Carriageway in  
accordance with  
local regulations

### Columns outside pit

#### Single Platform (EB)



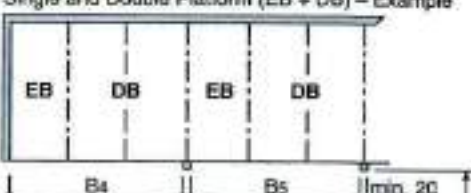
usable platform width	B4	B5
230	250	240
240	260	250
250	270	260
260	280	270
270	290	280

#### Double Platform (DB)



usable platform width	B4	B5
460	480	470
470	490	480
480	500	490
490	510	500
500	520	510
510	530	520
520	540	530
530	550	540
540	560	550

#### Single and Double Platform (EB + DB) – Example



usable platform width	B4	B5
230 + 460	740	730
240 + 470	760	750
250 + 480	780	770
260 + 500	800	790
270 + 500	820	810
270 + 510	830	820
270 + 520	840	830
270 + 530	850	840
270 + 540	860	850

Carriageway in  
accordance with  
local regulations

**!** For parking boxes on the edges and boxes with intermediate walls we recommend our maximum platform width of 270 cm for single platforms and 540 for double platforms. Problems may occur if smaller platform widths are used (depending on car type, access and individual driving behaviour and capability).

For larger limousines and SUV wider driveways are necessary (in particular on the boxes on the sides due to the missing manoeuvring radius).



Seite 1  
Section  
Dimensions  
Car data

Seite 2  
Width dim.  
without door

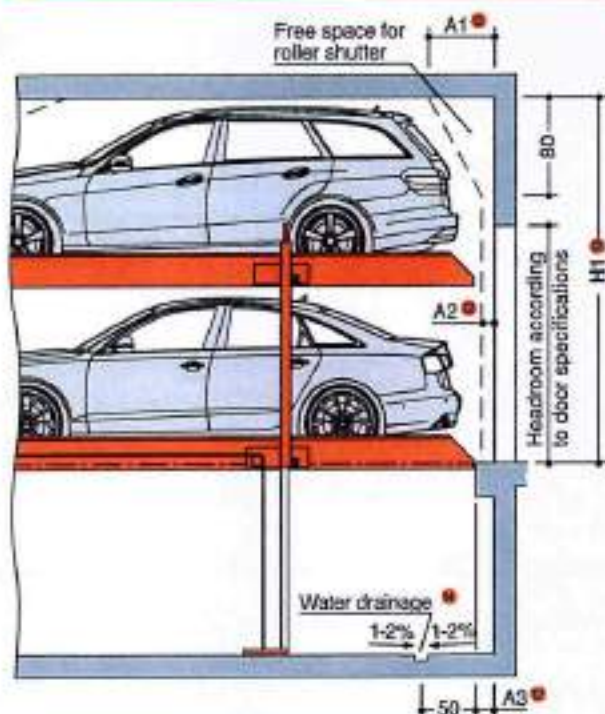
Seite 3  
Width dim.  
with door  
Function  
Approach

Seite 4  
Load plan  
Installation

Seite 5  
Electrical  
installation  
Technical  
data

Seite 6  
To be performed by the customer  
Description

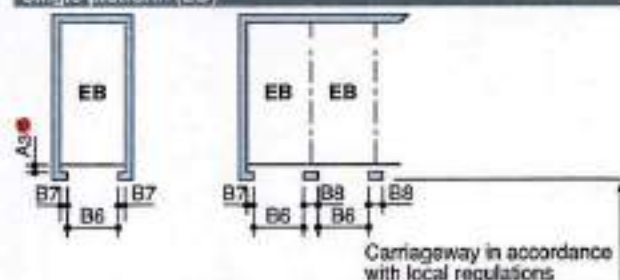
## Garage with door



- Dimensions A1, A2 and A3 must be coordinated with the door supplier (provided by the customer).
- See Page 1.
- Slope with drainage channel and sump.
- Seat-engaging surface (dimensions require coordination with door supplier.) Allround door dimensions require coordination between door supplier and local agency of KLAUS Multiparking.

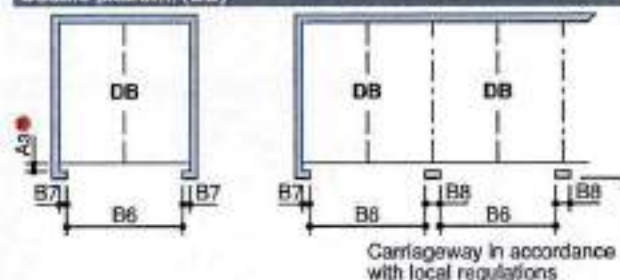
## Width dimensions for garage with door

### Single platform (EB)



usable platform width	door entrance width B6	B7	B8
230	230	15	30
240	240	15	30
250	250	15	30
260	260	15	30
270	270	15	30

### Double platform (DB)



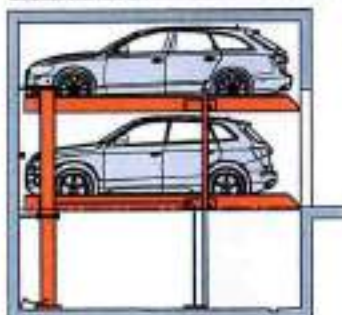
usable platform width	door entrance width B6	B7	B8
460	460	15	30
470	470	15	30
480	480	15	30
490	490	15	30
500	500	15	30
510	510	15	30
520	520	15	30
530	530	15	30
540	540	15	30

- ! For parking boxes on the edges and boxes with intermediate walls we recommend our maximum platform width of 270 cm for single platforms and 540 for double platforms. Problems may occur if smaller platform widths are used (depending on car type, access and individual driving behaviour and capability).

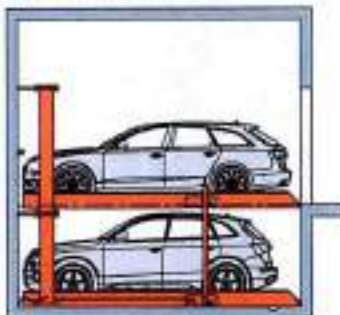
For larger limousines and SUV wider driveways are necessary (in particular on the boxes on the sides due to the missing manoeuvring radius).

## Funktion

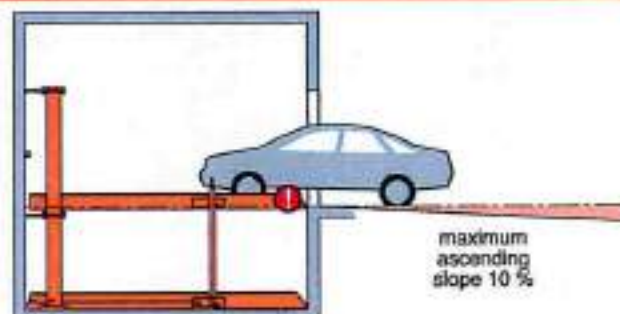
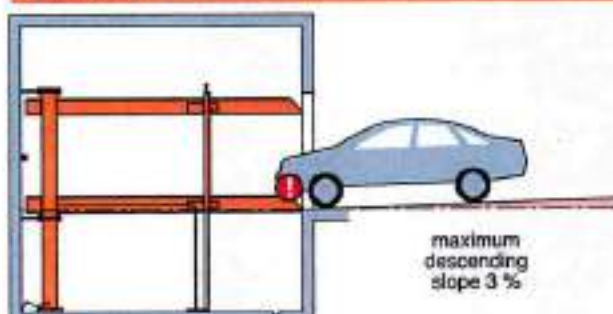
### System lifted



### System lowered



## Approach



- ! The illustrated maximum approach angles must not be exceeded. Incorrect approach angles will cause serious manoeuvring & positioning problems on the parking system for which the local agency of KLAUS Multiparking accepts no responsibility.

Seite 1  
Sektion  
Dimensions:  
Car data

Seite 2  
Width dim.  
without door

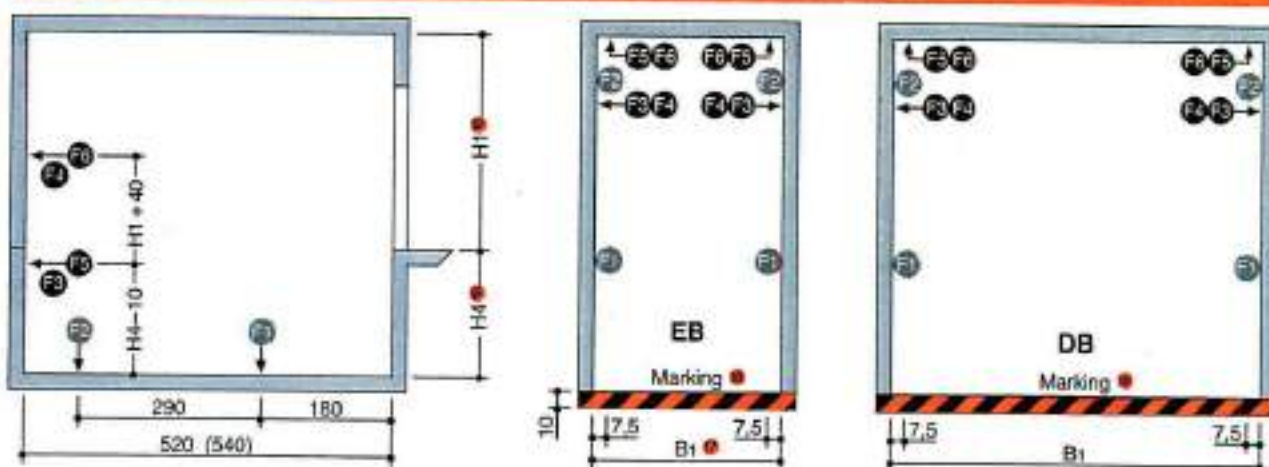
Seite 3  
Width dim.  
with door  
Function:  
Approach

Seite 4  
Load plan  
Installation

Seite 5  
Electrical  
Installation  
Technical  
data

Seite 6  
To be performed  
by the  
customer  
Description

## Load plan



platform load	F1	F2	F3	F4	F5	F6
EB 2000 kg	+25 -2	+12	±1	±0,8	±1,1	±1,1
EB 2500 kg	+30 -2,5	+15	±1,3	±1	±1,4	±1,4
DB 2000 kg	+51 -7,9	+20	±1,6	±2,6	±2	±2
DB 2500 kg	+57 -10	+25	±2,1	±3,4	±2,6	±2,6



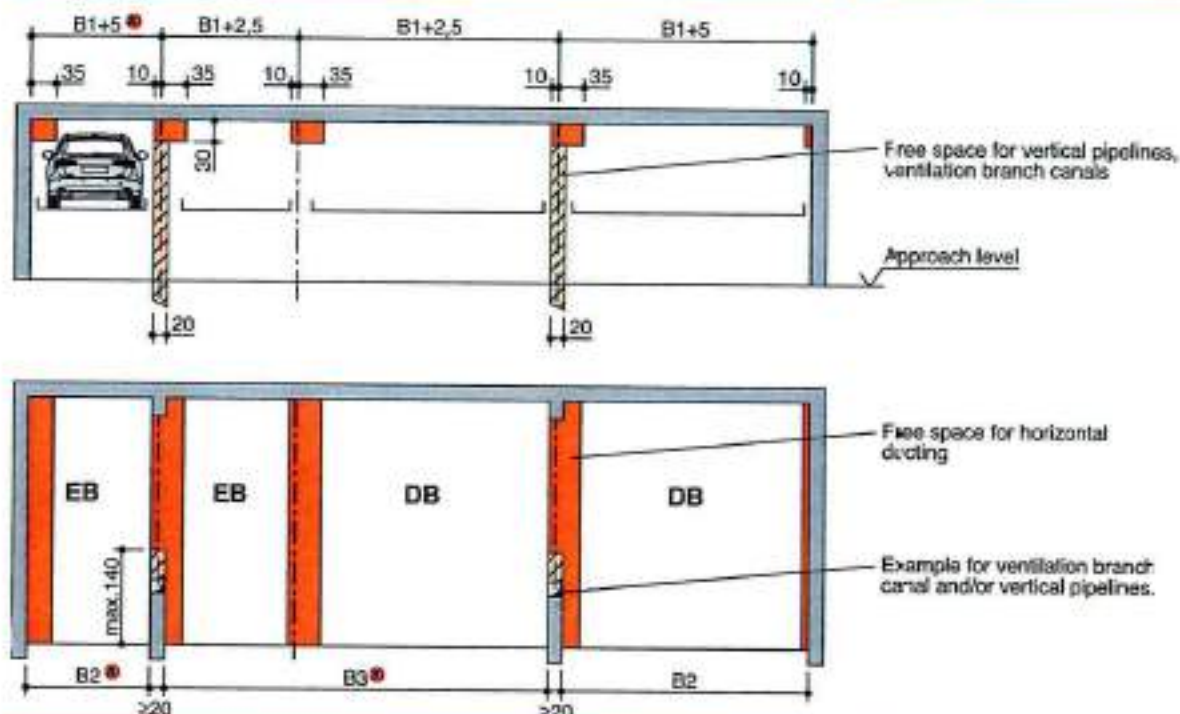
Units are dowelled to the floor, Drilling depth: approx. 15 cm.

Floor and walls below the drive-in level are to be made of concrete (quality minimum C20/25)

The dimensions for the points of support are rounded values. If the exact position is required, please contact KLAUS Multiparking.

- Dimensions H1 and H4 see page 1
- Dimension B1 see page 2
- Marking compliant to ISO 3864 (colors used in this illustration are not ISO 3864 compliant)
- All forces in kN

## Installation data – Free space for longitudinal and vertical ducts (e.g. ventilation)



Free space only applicable if vehicle is parked forwards = FRONT FIRST and driver's door on the left side.

- Dimensions B1, B2 and B3 see page 2



Seite 1  
Section  
Dimensions  
Car data

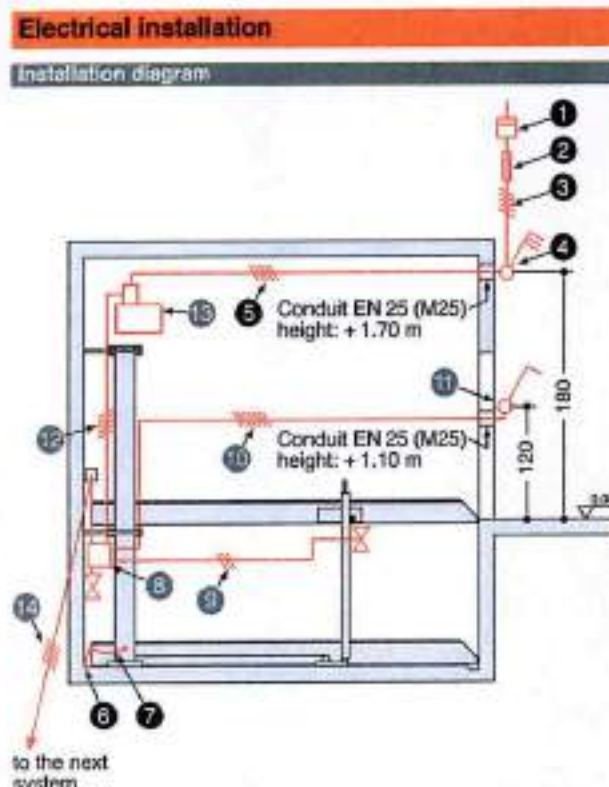
Seite 2  
Width dim.  
without door

Seite 3  
Width dim.  
with door  
Function  
Approach

Seite 4  
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Seite 5  
Electrical  
Installation  
Technical  
data

Seite 6  
To be performed  
by the customer  
Description



## Electrical installation

### Installation diagram

### Electrical data (to be performed by the customer)

No.	Quantity	Description	Position	Frequency
1	1	Electricity meter	in the supply line	
2	1	Main fuse: 3 x fuse 16 A (slow) or circuit breaker 3 x 16 A (bigger characteristic K or C) 3 x fuse 20 A (slow) or circuit breaker 3 x 20 A (bigger characteristic K or C)	in the supply line	1 per 3,0 kW unit 1 per 5,2 kW unit
3	1	Supply line 5 x 2.5 mm <sup>2</sup> (3 PH + N + PE) with marked wire and protective conductor	to main switch	1 per unit
4	1	Lockable main switch	defined at the plan evaluation	1 per unit
5	1	Supply line 5 x 2.5 mm <sup>2</sup> (3 PH + N + PE) with marked wire and protective conductor	from main switch to unit	1 per unit
6	every 10 m	Foundation earth connector	corner pit floor	
7	1	Equipotential bonding in accordance with DIN EN 60364 from foundation earth connector to the system		1 per system

### Electrical data (included in delivery of KLAUS Multiparking)

No.	Description
8	Terminal box
9	Control line 3 x 0.75 mm <sup>2</sup> (PH + N + PE)
10	Control line 7 x 1.5 mm <sup>2</sup> with marked wire and protective conductor
11	Operating device
12	Control line 5 x 1.5 mm <sup>2</sup> with marked wire and protective conductor
13	Hydraulic unit 3,0 kW/5,2 kW, three-phase current, 400 V / 50 Hz
14	Control line 5 x 1.5 mm <sup>2</sup> with marked wire and protective conductor

● Unit with 5,2 kW only for 2072 DB 2.6 to

## Technical data

### Field of application

By default, the system can only be used for a fixed number of users.

If different users use the system – only on the upper parking spaces – (e.g. short-time parkers in office buildings or hotels) the Multiparking system needs to be adjusted. If required, would you please contact us.

### Units

Low-noise power units mounted to rubber-bonded-to metal mountings are installed. Nevertheless we recommend that parking system's garage be built separately from the dwelling.

### Available documents

- wall recess plans
- maintenance offer/contract
- declaration of conformity
- test sheet on airborne and slid-borne sound

### Building application documents

According to LBO and GaVo (garage regulations) the Multiparking systems are subject to approval. We will provide the required building application documents.

### Corrosion protection

See separate sheet regarding corrosion protection.

### Care

To avoid damages resulting from corrosion, make sure to follow our cleaning and care instructions and to provide good ventilation of your garage.

### Railings

If the permissible drop opening is exceeded, railings are to be mounted on the systems. If there are traffic routes next to or behind the installations, railings compliant to DIN EN ISO 13857 must be installed by the customer. Railings must also be in place during construction.

### Environmental conditions

Environmental conditions for the area of multiparking systems: Temperature range -10 to +40° C. Relative humidity 50% at a maximum outside temperature of +40° C.

If lifting or lowering times are specified, they refer to an environmental temperature of +10° C and with the system set up directly next to the hydraulic unit. At lower temperatures or with longer hydraulic lines, these times increase.

### CE Certification

The systems offered correspond to DIN EN 14010 and the EC Machinery Directive 2006/42/EG.

### Sound insulation

According to DIN 4109 (Sound insulation in buildings), para. 4, annotation 4, KLAUS Multiparkers are part of the building services (garage systems).

### Normal sound insulation:

DIN 4109, para. 4, Sound insulation against noises from building services.

Table 4 in para. 4.1 contains the permissible sound level values emitted from building services for personal living and working areas. According to line 2 the maximum sound level in personal living and working areas must not exceed 30 dB (A).

Noises created by users are not subject to the requirements (see table 4, DIN 4109).

The following measures are to be taken to comply with this value:

- Sound protection package according to offer/order (KLAUS Multiparking GmbH)
- Minimum sound insulation of building  $R'_{w} = 57$  dB (to be provided by customer)

### Increased sound insulation (special agreement):

Draft DIN 4109-10, information on planning and execution, proposals for increased sound insulation.

Agreement: Maximum sound level in personal living and working areas 25 dB (A). Noises created by users are not subject to the requirements (see table 4, DIN 4109).

The following measures are to be taken to comply with this value:

- Sound protection package according to offer/order (KLAUS Multiparking GmbH)
- Minimum sound insulation of building  $R'_{w} = 62$  dB (to be provided by customer)

Note: User noises are noises created by individual users in our Multiparking systems. These can be noises from accessing the platforms, slamming of vehicle doors, motor and brake noises.



Seite 1	Section
Dimensions	Car data
Seite 2	Width dim. without door
Seite 3	Width dim. with door
Function	Approach
Seite 4	Load plan
Installation	
Seite 5	Electrical installation
Technical data	
Seite 6	To be performed by the customer
Description	

## To be performed by the customer

### Safety fences

Any constraints that may be necessary according to DIN EN ISO 13857 in order to provide protection for the park pits for pathways directly in front, next to or behind the unit. This is also valid during construction. Railings for the system are included in the series delivery when necessary.

### Numbering of parking spaces

Consecutive numbering of parking spaces.

### Building services

Any required lighting, ventilation, fire extinguishing and fire alarm systems as well as clarification and compliance with the relevant regulatory requirements.

### Drainage

For the front area of the pit we recommend a drainage channel, which you connect to a floor drain system or sump (50 x 50 x 20 cm). The drainage channel may be inclined to the side, however not the pit floor itself (longitudinal incline is available). For reasons of environmental protection we recommend to paint the pit floor, and to provide oil and petrol separators in the connections to the public sewage network.

### Strip footings

If due to structural conditions strip footings must be effected, the customer shall provide an accessible platform reaching to the top of the said strip footings to enable and facilitate themounting work.

### Marking

According to DIN EN 14010, a warning that identifies this danger area must be placed in the entrance area that conforms to ISO 3884. This must be done according to EN 92/59/EEG for systems with a pit (platforms within the pit) 10 cm from the edge of the pit.

### Wall cuttings

Any necessary wall cuttings according to page 1.

### Electrical supply to the main switch / Foundation earth connector

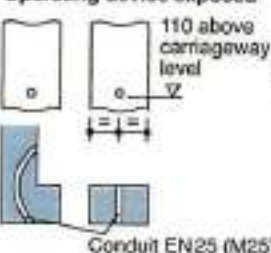
Suitable electrical supply to the main switch and the control wire line must be provided by the customer during installation. The functionality can be monitored on site by our fitters together with the electrician. If this cannot be done during installation for some reason for which the customer is responsible, the customer must commission an electrician at their own expense and risk.

In accordance with DIN EN 60204 (Safety of Machinery, Electrical Equipment), grounding of the steel structure is necessary, provided by the customer (distance between grounding max. 10 m).

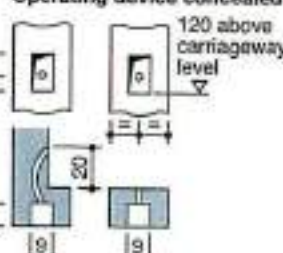
### Operating device

Cable conduits and recesses for operating device (for double wing doors: please contact the local agency of KLAUS Multiparking).

#### Operating device exposed



#### Operating device concealed



If the following are not included in the quotation, they will also have to be provided / paid for by the customer:

- Mounting of contactor and terminal box to the wall valve, complete wiring of all elements in accordance with the circuit diagram
- Costs for final technical approval by an authorized body
- Main switch
- Control line from main switch to hydraulic unit

## Description Single platform (EB) and Double platform (DB)

### General description

Multiparking system providing independent parking spaces for 2 cars (EB), 2 x 2 cars (DB), one on top of the other each.

Dimensions are in accordance with the underlying dimensions of parking pit, height and width.

The parking bays are accessed horizontally (installation deviation  $\pm 1\%$  for correct drainage of platforms).

Due to the special lifting and bearing construction lifting of the doors is not restricted.

Vehicles are positioned on each parking space using wheel stops on the right side (adjust according to operating instructions).

Operation via operating device with hold-to-run-device using master keys.

The operating elements are usually mounted either in front of the column or on the outside of the door frame.

Operating instructions are attached to each operator's stand.

For garages with doors at the front of the parking system the special dimensional requirements have to be taken into account.

### Multiparking system consisting of:

- 2 steel pillars (mounted on the floor)
- 2 sliding platforms (mounted to the steel pillars with sliding bearings)
- 2 platforms
- 1 mechanic synchronization control system (to ensure synchronous operation of the hydraulic cylinders while lowering and lifting the platform)
- 2 hydraulic cylinders
- 2 rigid supports (connect the platforms)
- 2 chains and pocket wheels
- 1 automatic hydraulic safety valve (prevents accidental lowering of the platform while accessing the platform)
- Dowels, screws, connecting elements, bolts, etc.
- The platforms and parking spaces are end-to-end accessible for parking!

### Platforms consisting of:

- Platform base sections
- Adjustable wheel stops
- Cantilever access plates
- Side members
- Central side member (only DB)
- Cross members (DB long and short cross members)
- Safety railings - along the upper and lower platform (if required)
- Screws, nuts, washers, distance tubes, etc.

### Hydraulic system consisting of:

- Hydraulic cylinder
- Solenoid valve
- Safety valve
- Hydraulic conduits
- Screwed joints
- High-pressure hoses
- Installation material

### Electric system consisting of:

- Operating device (Emergency Stop, lock, 1 master key per parking space)
- Terminal box at wall valve

### Hydraulic unit consisting of:

- Hydraulic power unit (low-noise, installed onto a console with a rubber-bonded-to-metal mounting)
- Hydraulic oil reservoir
- Oil filling
- Internal geared wheel pump
- Pump holder
- Clutch
- 3-phase-AC-motor
- Contactor (with thermal overcurrent relay and control fuse)
- Test manometer
- Pressure relief valve
- Hydraulic hoses (which reduce noise transmission onto the hydraulic pipe)

## We reserve the right to change this specification without further notice

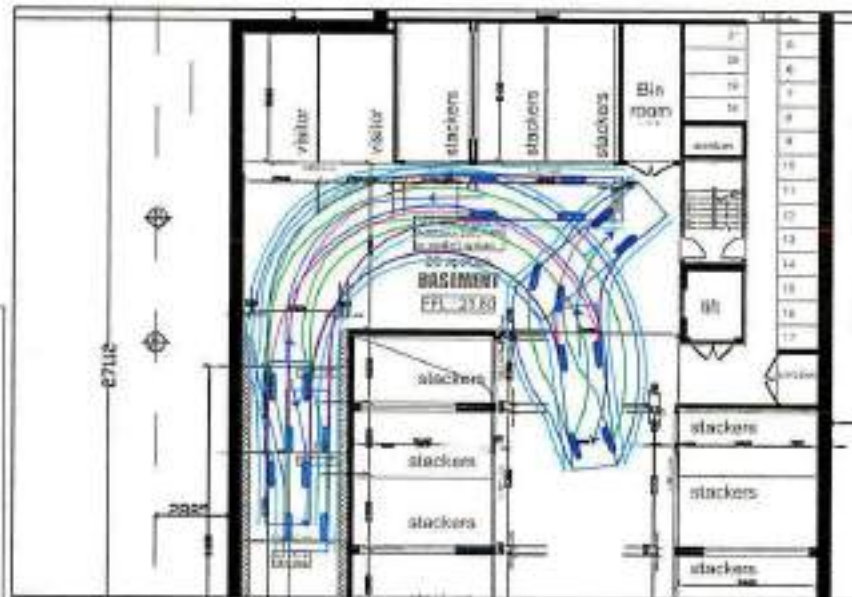
KLAUS Multiparking reserves the right in the course of technical progress to use newer or other technologies, systems, processes, procedures or standards in the fulfillment of their obligations other than those originally offered provided the customer derives no disadvantage from their so doing.



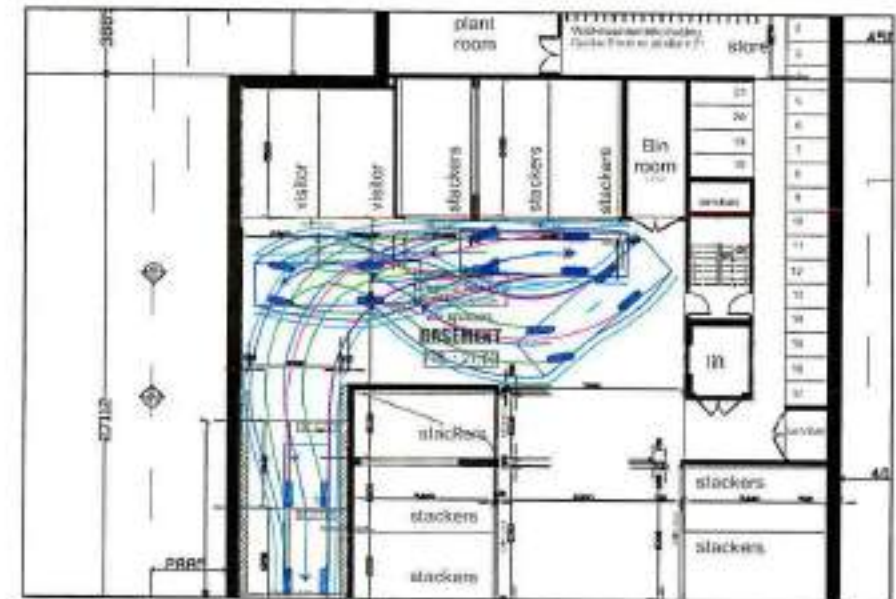


## Appendix D: Swept Path Diagrams

WASTE WISE MINI - INGRESS



WASTE WISE MINI - EGRESS



VEHICLE USED IN SIMULATION  
(VEHICLE SPEED - 3000 RPM)



Waste Wise Mini Loader mm  
Width : 1200  
Track : 1800  
Lock to Lock Time : 6.0  
Steering Angle : 33.5


WASTE WISE MINI - EGRESS



LEGEND  
BACKSWING  
TRAVEL  
SHUTTLE  
SORT/CLEARANCE



PRELIMINARY ONLY  
NOT FOR CONSTRUCTION

REV.	REVISION/NOTES	APPROVED DATE	APPROVED SIGNATURE	DESIGNED BY: D. ARJUN	8 DEC 2018	 Traffic Engineering and Transport Planning 134-138 MCKINNON ROAD, MCKINNON B85 DESIGN CAR SWEEP PATHS PROPOSED RESIDENTIAL DEVELOPMENT
				CHECKED BY: D. SUDHAKAR	8 DEC 2018	
				DRAWN BY: G. SUDHAKAR	8 DEC 2018	
				SCALE: AS SHOWN	AS SHOWN	